

FY 2026 Municipal Community Mitigation Fund Grant Application

Application Instructions:

- I. All applications must be received by the Massachusetts Gaming Commission by January 31st, 2025, at 11:59 p.m. to be considered for funding for the FY 2026 grant round.
- II. Each municipality may only submit **ONE** application as a Word Document.
- III. Each project must have its own form within the appropriate category. Forms can be found below as Parts A-E. If there is more than one project in a category, please copy the form. All attachments should directly follow the relevant project form.
- IV. Be sure to fill in all the information requested on the application. Applications that are left incomplete will not be accepted.
- V. All applications must submit a detailed scope of work and timeline for implementation for all projects identified in the application.
- VI. All applications must contain appropriate backup materials that support the application.
- VII. The Municipal Grant Manager will be the person responsible for compiling the information for the quarterly reports. The application must be signed by the municipal administrator or an individual with signatory authority. Submit this completed form as well as any relevant attachments to <u>MGCCMF@Massgaming.gov</u> or as a response to the COMMBUYS BID BD25-1068- 1068C-1068L-109685

For more detailed instructions as well as the full FY 2026 Application Guidelines

visit https://massgaming.com/about/community-mitigation-fund/

Municipal Grant Manager Information:

Applicant: The City of Boston

Vendor Code: VC6000192075

Name: Marcus Kennedy

Title: Director of External Resource Development-Office of Budget Management

Email Address: marcus.kennedy@boston.gov

Telephone:617-635-3838

Address: 1 City Hall Square, Room 813, Boston, MA 02201

Grant Budget Summary

Your community's FY 2026 proposed allocation can be found at

<u>https://massgaming.com/about/community-mitigation-fund/</u>. Use the space below to total all requests by category. Please clarify how many discreet projects your community plans to undertake per category.

Total FY 2026 Allocation:\$2,607,000			
Application Totals by Category	# of	Requested	
··••••••••••••••••••••••••••••••••••••	Projects	Amount	
A. Community Planning			
B. Public Safety		\$758,837.54	
C. Transportation		\$1,848,162.46	
D. Gambling Harm Reduction			
E. Specific Impact			
TOTAL		\$2,607,000.00	

- I. Are you requesting a waiver for any program requirement?
- II. Yes
- III. If yes, you must fill out a CMF Municipal Waiver Form. The waiver form can be found as Appendix E to the RFR on COMMBUYS or online at <u>https://massgaming.com/about/community-mitigation-fund/forms/</u>. Applications without a completed waiver form will not be considered for a waiver.

Budget Category Summary

Use the space below to provide an overview of all projects to be covered by this funding. You may add as many items as is pertinent to your application (you can add rows by right clicking on the row and selecting "add row"). Please provide a category, name, brief description, and amount for each item.

Category	Project Name	Description	Amount
A. Community	NA	NA	NA
Planning			
B. Public Safety	Boston Police	See Scope	\$758,837.54
	Department		
C. Transportation	Chelsea St at	This traffic improvement project constructs a new	\$748,162.46
	Terminal St	signal intersection to manage the existing truck traffic	

	Intersection Improvements	along with the increased volumes of pedestrian traffic. The City is proposing a new accessible crossing to accommodate the increase in foot traffic. Currently, there is not an existing accessible crosswalk at this location	
C. Transportation	Bikeshare Expansion in Charlestown	This project would allow the City of Boston to purchase and install 4 additional bikeshare stations with bikes in Charlestown at critical locations to help ensure that we mitigate the impact of congestion and traffic from the Encore Casino.	\$200,000.00
C. Transportation	Boston Harborwalk Extension Construction	Construct a missing Boston Harborwalk section in the vicinity of Encore Boston Harbor, improving roadway safety, and traffic congestion.	\$900,000.00
D. Gambling Harm	NA	NA	NA
Reduction			
E. Specific Impact	NA	NA	NA

Applicant

On behalf of the aforementioned applicant, I hereby certify that the funds that are requested in this application will be used solely for the purposes articulated in this application.

Johanna Bernstein

1/30/2025

Signature:

Johanna Bernstein Deputy Director Operating-OBM

Name and Title of Signatory:

Date:

Part A-Community Planning - The application should include sufficient backup information for the review team to fully understand the project(s). This information could include locus maps, requests for proposals, detailed scopes of work, drawings etc.

Project Name:		
Please provide below the contact information for the individual managing this aspect of the grant.		
Project Contact Additional Project Contact (if applicable		
Name:	Name:	
Title:	Title:	
Department:	Department:	
Email Address:	Email Address:	
Telephone:	Telephone:	
Address: Address:		

I. Please use the space below to identify the impact of the gaming establishment on your municipality. You may use the impacts identified in the FY 2026 Guidelines relevant to this category. If you are using an impact not identified in the guidelines, please use the space below to identify the impact. Please provide documentation or evidence that gives support for the determination that the operation of the gaming facility caused or is causing the impact (i.e., surveys, data, reports, etc.)

II. Please describe the project in detail and how the proposed project will address the impact indicated above. Please include a breakdown of the proposed scope of work, the scope should be sufficiently detailed to allow the review team to understand the steps required for project completion.

Proposed MGC Grant Budget

Please use the following table to outline the project budget. Please include as an attachment any requests for proposals, quotes, or estimates that would quantify the costs associated with the mitigation.

Description of Purchase/Work	Timeline	QTY	Budget
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Part B- Transportation The application should include sufficient backup information for the review team to fully understand the project(s). This information could include locus maps, requests for proposals, detailed scopes of work, etc.

Project Name: Chelsea Street at Terminal Street Intersection Safety Improvements

Please provide below the contact information for the individual managing this aspect of the grant. If there are multiple people involved in the grants management, please add more lines to accommodate their information.

Project Contact:	Additional Project Contact (if applicable)	
Name: Jeff Alexis	Name:	
Title: Deputy Director of Engineering	Title:	
Department: Public Works	Department:	
Email Address: jeffrey.alexis@boston.gov	Email Address:	
Telephone:	Telephone:	
Address: One City Hall Plaza, Boston, MA 02201	Address: One City Hall Plaza, Boston, MA 02201	

I. Please use the space below to identify the impact of the gaming establishment on your municipality. You may use the impacts identified in the FY 2026 Guidelines relevant to this category. If you are using an impact not identified in the guidelines, please use the space below to identify the impact. Please provide documentation or evidence that gives support for the determination that the operation of the gaming facility causes or is causing the impact (i.e., surveys, data, reports, etc.)

The site of the Chelsea Street at Terminal Street Intersection Safety Improvements is bounded by Route 1 and Route 99, and in close proximity to Interstate 93. All three routes have been identified in the casino's Environmental Impact Report as carrying the following casino-related traffic (patron and employee composite trip distribution) for Encore Boston Harbor:

- I-93 South: 37%
- I-93 North: 15%
- Route 99 North: 2%
- Route 1 North: 1%

The Project will address the following identified transportation-related impacts:

- Increased traffic associated with the gaming establishment may cause increased congestion on the major routes leading to/from the gaming establishment. In the City of Boston, in and around Charlestown, impacted streets include: Route 99 (Rutherford Ave/Alford St), Cambridge St, Main St, Medford St, Bunker Hill St, and other neighborhood side streets in the vicinity of the Casino. In a community-engaged research study funded by the Mass Gaming Commission, "many participants who reported driving past Encore on Route 99 noted that its presence increased traffic in the area, resulting in longer commute times and increased congestion" (NORC-ICH, 2022).
- Increased traffic associated with the gaming establishment may result in increased vehicular/bicycle/pedestrian conflicts. Figure 1 below shows the 26 pedestrian-related crash locations that resulted in injuries or fatalities within a 1.5m radius of the casino in Boston, from

6/23/2019 to 1/30/2024; four of those were fatalities.



Figure 2 below shows the 48 cyclist-related crash locations that resulted in injuries or fatalities within a 1.5m radius of the casino in Boston, from 6/23/2019 to 1/30/2024.



- Increased traffic associated with the gaming establishment may cause localized increases in air pollution due to congestion. In Charlestown, increased traffic on the routes noted will contribute to localized pollution in the neighborhood due to congestion.
- Increased visitation to the gaming establishment area has placed a strain on public transit services. Relevant MBTA services in Charlestown include Sullivan Square Station and the 109 bus route.

Environmental Protection Agency (2025). EJScreen Community Report. GREO (2023). Presentation of Evidence for Massachusetts Gaming Commission: Impacts of casino gambling on priority populations. NORC at the University of Chicago, Institute for Community Health (2022). Community Perspectives on Encore Boston Harbor Casino: Final Report.

II. Please describe the project in detail and how the proposed project will address the impact indicated above. Please include a breakdown of the proposed scope of work, the scope should be sufficiently detailed to allow the review team to understand the steps required for project completion.

The Chelsea Street at Terminal Street design is a traffic improvement project where we are introducing signal control to manage the existing truck traffic along with the increased volumes of pedestrian traffic. Complementing the proposed extension of the Harborwalk (separate request below), the City is proposing a new accessible crossing to accommodate the increase in foot traffic. Currently, there is not an existing accessible crosswalk at this location.

This project is currently at final PS&E design and has an estimate of \$1.91m, which includes a standard 15% construction contingency and a bid contingency based on volatility we've been seeing in the market. The project design and estimate are attached to this application.

Description of Purchase/Work	Timeline	QTY	Budget
Chelsea Street at Terminal Street Intersection Safety Improvements	July 2025 - June 2027		\$748,162.46
	TOTAL:		\$748,162.46

Project Name: Bikeshare Expansion in Charlestown		
Please provide below the contact information for the individual managing this aspect of the grant. If		
there are multiple people involved in the grants m	anagement, please add more lines to	
accommodate their information.		
Project Contact: Additional Project Contact (if applicable)		
Name: Kim Foltz Name:		
Title: Boston Bikes Director Title:		
Department: Transportation Dept Department:		
Email Address: kim.foltz@boston.gov Email Address:		
Telephone:	Telephone:	

Address: One City Hall Plaza, Boston, MA 02201 Address: One City Hall Plaza, Boston, MA 02201

I. Please use the space below to identify the impact of the gaming establishment on your municipality. You may use the impacts identified in the FY 2026 Guidelines relevant to this category. If you are using an impact not identified in the guidelines, please use the space below to identify the impact. Please provide documentation or evidence that gives support for the determination that the operation of the gaming facility causes or is causing the impact (i.e., surveys, data, reports, etc.)

The Project will address the following identified transportation-related impacts:

- Increased traffic associated with the gaming establishment may cause increased congestion on the major routes leading to/from the gaming establishment. In the City of Boston, in and around Charlestown, impacted streets include: Route 99 (Rutherford Ave/Alford St), Cambridge St, Main St, Medford St, Bunker Hill St, and other neighborhood side streets in the vicinity of the Casino. In a community-engaged research study funded by the MassGaming Commission, "many participants who reported driving past Encore on Route 99 noted that its presence increased traffic in the area, resulting in longer commute times and increased congestion" (NORC-ICH, 2022).
- Increased traffic associated with the gaming establishment may result in increased vehicular/bicycle/pedestrian conflicts. Figure 1 below shows the 26 pedestrian-related crash locations that resulted in injuries or fatalities within a 1.5m radius of the casino in Boston, from 6/23/2019 to 1/30/2024; four of those were fatalities.

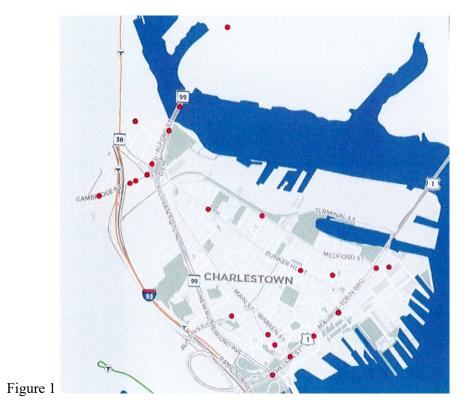
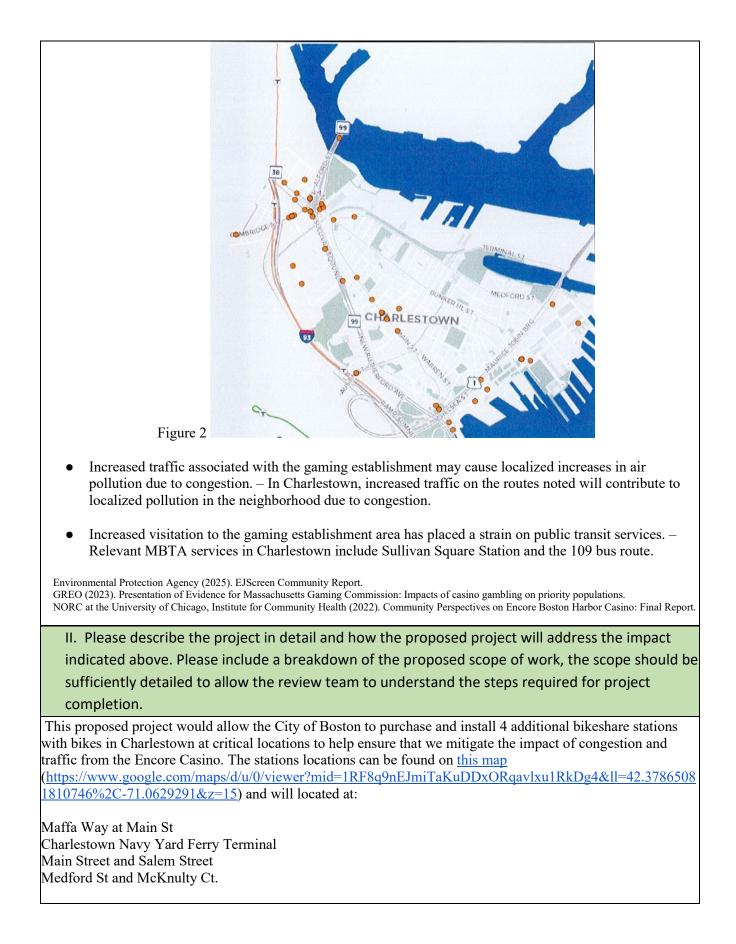


Figure 2 below shows the 48 cyclist-related crash locations that resulted in injuries or fatalities within a 1.5m radius of the casino in Boston, from 6/23/2019 to 1/30/2024.



This complements our expansion throughout the neighborhood and Charlestown and helps us reduce the demand for cars and parking that has impacted this neighborhood after the construction of the casino.

Description of Purchase/Work	Timeline	QTY	Budget
A 19-dock station with bikes	July 2025 - July 2026	4	\$50,000
	TOTAL:	4	\$200,000

Project Name: Boston Harborwalk Extension Construction

Please provide below the contact information for the individual managing this aspect of the grant. If there are multiple people involved in the grants management, please add more lines to accommodate their information.

Project Contact:	Additional Project Contact (if applicable)		
Name: <u>William Epperson</u>	Name: Gamze Munden		
Title: Deputy Director for Capital Construction	Title: Senior Project Engineer		
Department: Planning Department	Department: Planning Department		
Email Address: william.j.epperson@boston.gov	Email Address: gigi.munden@gmail.com		
Telephone: 617-918-6202	Telephone: 617-918-6203		
Address: One City Hall Plaza, Boston, MA 02201	Address: One City Hall Plaza, Boston, MA 02201		

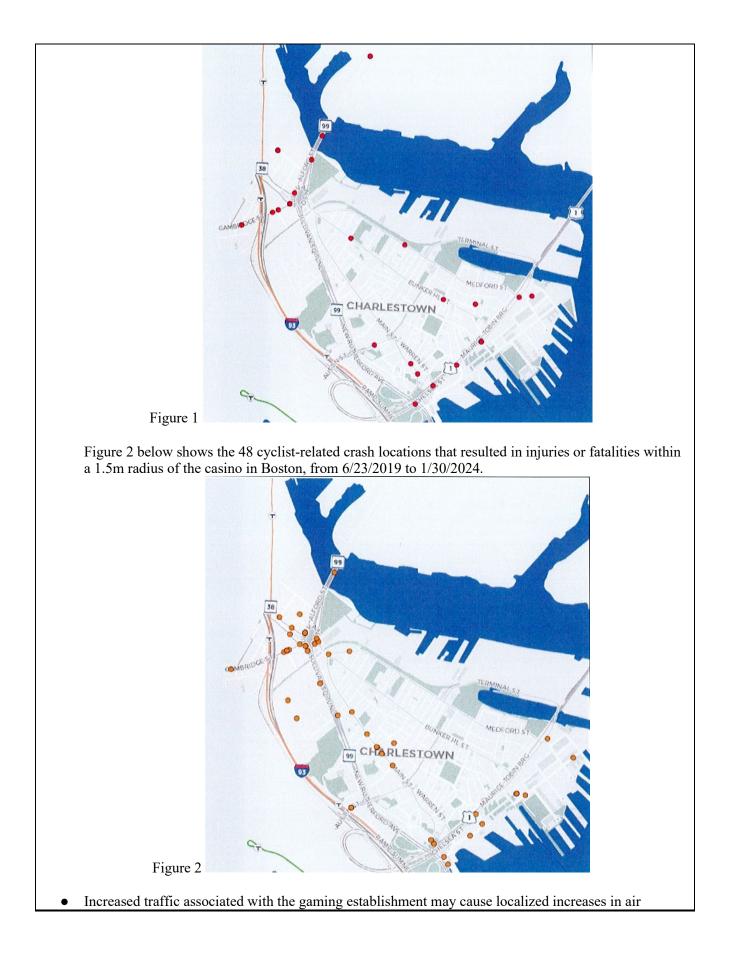
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The site of the Boston Harborwalk Extension Construction project is bounded by Route 1 and Route 99, and in close proximity to Interstate 93. All three routes have been identified in the casino's Environmental Impact Report as carrying the following casino-related traffic (patron and employee composite trip distribution) for Encore Boston Harbor:

- I-93 South: 37%
- I-93 North: 15%
- Route 99 North: 2%
- Route 1 North: 1%

The Project will address the following identified transportation-related impacts:

- Increased traffic associated with the gaming establishment may cause increased congestion on the major routes leading to/from the gaming establishment. In the City of Boston, in and around Charlestown, impacted streets include: Route 99 (Rutherford Ave/Alford St), Cambridge St, Main St, Medford St, Bunker Hill St, and other neighborhood side streets in the vicinity of the Casino. In a community-engaged research study funded by the MassGaming Commission, "many participants who reported driving past Encore on Route 99 noted that its presence increased traffic in the area, resulting in longer commute times and increased congestion" (NORC-ICH, 2022).
- Increased traffic associated with the gaming establishment may result in increased vehicular/bicycle/pedestrian conflicts. Figure 1 below shows the 26 pedestrian-related crash locations that resulted in injuries or fatalities within a 1.5m radius of the casino in Boston, from 6/23/2019 to 1/30/2024; four of those were fatalities.



pollution due to congestion. – In Charlestown, increased traffic on the routes noted will contribute to localized pollution in the neighborhood due to congestion.

 Increased visitation to the gaming establishment area has placed a strain on public transit services. – Relevant MBTA services in Charlestown include Sullivan Square Station and the 109 bus route.

Additionally, according to a presentation of evidence to the Mass Gaming Commission by GREO, East & South Asian populations in Massachusetts generally display a higher risk for problem gambling compared to non-Asian populations; social integration challenges among Asian immigrants have been found to contribute to feelings of boredom and social isolation, which cause some to turn to gambling. Poverty, a significant root cause of problem gambling particularly among Asian immigrants, further compounds these issues. (GREO 2023). The block group within which the Project Site is located is home to significant low-income and immigrant populations—76% low income and 79% people of color, where 54% are Asian and 26% are Hispanic; 12% of households speak limited English, with 100% of these limited English households speaking Asian-Pacific Island languages (EPA EJScreen 2025). Alternative, localized recreational activities help to increase engagement and reduce risks of gambling harms for priority populations living adjacent to the project site.

II. Please describe the project in detail and how the proposed project will address the impact indicated above. Please include a breakdown of the proposed scope of work, the scope should be sufficiently detailed to allow the review team to understand the steps required for project completion.

The project will mitigate increased traffic produced by Encore Boston Harbor by constructing an extension of the Boston Harborwalk, a celebrated shared-use path system along the Boston waterfront providing a safe, continuous connection with Encore Boston Harbor for vulnerable road users. The project is located along the route between Encore Boston Harbor and the Charlestown Navy Yard, which are two of the region's major tourism destinations. Providing an alternative route/mode for visitors to access these frequented destinations, as well as for local residents to access other area sites—will help relieve the congestion in Charlestown, and thereby mitigate localized increases in traffic-related air pollution, as well as reduce the strain on public transit services.

The Harborwalk Extension will complement other complete streets projects connecting to the project site, helping to offset traffic congestion in Charlestown and provide safe accommodations for vulnerable road users in a census block that is in the 99th state percentile for proximity to traffic and diesel particulate matter (EPA 2025). The project is a direct outcome of a pedestrian death located at its eastern terminus in 2021.

The project features a paved asphalt shared-use path facility that will be approximately 1,000 feet in length, and 11 feet in width. The grade/terrain is virtually flat, save for a ramp proposed for the eastern terminus of the project site that will bring users up to the elevation of Chelsea Street. The ramp will meet ADA regulations and has a maximum grade of 8%. Plantings, seating, shade structures, and other amenities will enhance Harborwalk utilization for the enjoyment of residents (majority low-income) and visitors to the Greater Boston area. It contributes to the City's focus on enhancing connectivity along the waterfront through an improved Harborwalk system in its Imagine Boston 2030 plan.

The Harborwalk Extension will also provide increased access and connectivity to alternative recreational activities for priority populations identified as at risk of experiencing gambling harms, including Asian, Hispanic, and Latino populations. Alternative, localized recreational activities - no better exemplified by

Environmental Protection Agency (2025). EJScreen Community Report. GREO (2023). Presentation of Evidence for Massachusetts Gaming Commission: Impacts of casino gambling on priority populations. NORC at the University of Chicago, Institute for Community Health (2022). Community Perspectives on Encore Boston Harbor Casino: Final Report.

activities (walking, biking, rolling, exploring) conducted along the Boston Harborwalk - help to increase engagement and mitigate risks of gambling harms for priority populations living adjacent to the project site.

As the project is located on the Boston Harborwalk, signage will promote Boston Harborwalk branding and identity, as well as regulatory signage promoting responsible public use (hours of operation, etc.) and permitting information (e.g. Ch. 91). The City of Boston is working with a consultant team to complete appropriate design development and permitting for the project. The parcel is owned by the Boston Redevelopment Authority d/b/a Boston Planning and Development Agency ("BPDA"). It is bounded by a leased lot on Terminal Street to the north, Chelsea Street to the east, the Little Mystic Channel to the south, and the Little Mystic Public Access Ramp to the west.

Extensive and ongoing community engagement of area residents since 2017 informed conceptual design for the proposed project, and a community steering committee continues to meet every other month to help guide project development. Design development and permitting is currently underway and the project is expected to be fully designed and permitted by Spring 2026, at which point the project will be bid for construction. Permitting activities have begun and include: NOI Preparation and Review by the Boston Conservation Commission, USACE Permit (GP2), Chapter 91 (Waterway License), and Massachusetts Environmental Policy Act (MEPA).

The pre-75% cost estimate for construction is \$5.73m. The requested amount of \$900,000 will complement additional funding from the City of Boston and private foundation grants. Supplemental project documentation, including project maps, photos, plans, cost estimate, design/permitting schedule, and letters of support, are provided as an attachment.

Description of Purchase/Work	Timeline	QTY	Budget
Boston Harborwalk Extension Construction	FY26 & FY27	1	\$900,000
	TOTAL:		\$900,000

Part C- Public Safety - The application should include sufficient backup information for the review team to fully understand the project(s). This information could include locus maps, catalog cuts of proposed equipment purchases, quotes, training course syllabus, etc.

Project Name: Boston Police Department's FY 2026 Public Safety Strategy for Crime Mitigation at the Encore Casino

Please provide below the contact information for the individual managing this aspect of the grant.

Project Contact:	Additional Project Contact (if applicable)
Name: Maria Cheevers	Name: Jason Whyte
Title: Director	Title: Grants Administrator
Department: Office of Research and	Department: Office of Research and Development
Development	
Email Address: Maria.cheevers@pd.boston.gov	Email Address: Jason.Whyte@pd.boston.gov
Telephone: 617.343.5096	Telephone: 617.343.5096
Address: 1 Schroeder Plaza, 4th Floor, Boston,	Address: 1 Schroeder Plaza, 4th Floor, Boston, MA
MA 02120	02120

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Identified Impacts: The BPD will address all of the following impacts within our proposed strategies:

- Increased visitation and employment due to the casino will likely increase the interaction between public safety personnel and casino patrons and employees.
- It is recognized by law enforcement and the casino industry that certain types of crime may be attracted to casinos and other hospitality-related businesses. These may include human trafficking, money laundering, and drug trafficking. Other crimes that may be attributable to casinos include increased assaults, fraud, and property crimes.
- The presence of casinos has been demonstrated to cause an increase in cases of operating under the influence.
- Increases in traffic can cause increases in congestion, accidents, and vehicular/bicycle/pedestrian conflicts.
- The influx of visitors to a casino can result in an increase in calls for service and put pressure on local emergency services including emergency responders like police, fire departments and EMS. This could lead to increased needs for mutual aid.

For this section of our response to this question, we will only note two new sources of information, as within

the 2025 application, we had already noted several other research summaries.

1) During a recent search on Chat GPT (an artificial intelligence app), by one of the Department's District Captains, when prompted with the question, Chat GPT identified the following "Crime Associated with Casino Activities":

- Money laundering,
- Fraud and cheating
- Gambling addiction
- Drug trafficking
- Prostitution and human trafficking
- Loan sharking and extortion
- Violent crime, and
- Tax evasion

Each of these crime categories has a negative community impact, including; economic consequences, social consequences, physical and emotional harm, and quality of life consequences.

2) Impacts of casino gambling on public safety Sheila McKnight, Senior Knowledge Broker Lindsay Kalbfleisch, Director of Stakeholder Engagement Sasha Stark, PhD, Director of Research and Evidence Services Presentation of Evidence for Massachusetts Gaming Commission September 2023

Encore Boston Harbor Casino had few direct links with public safety issues during its first year and four months of operation (July 2019 to October 2020). Overall, the region and most cities had expected levels of crime. In its first six months of operation, there were increases in crime and calls for service that were not unexpected given the number of people who were drawn to the casino (more than 3.5 million visitors during the first eight months). In Everett, not accounting for any surrounding community impact, this resulted in a 9.5% increase in violent crime, a 5.1% increase in property crime, a 6% increase in total crime, and a 4.1% increase in calls for service. In its first six months of operation, Encore Boston Harbor casino itself accounted for 124 arrests, including 22 violent crimes and 20 property crimes. Increases occurred in some public safety issues across the wider region surrounding the casinos in the early months and years they were open. It is unclear if these increases are directly linked to the presence of casinos, unless otherwise indicated. These included drunk driving, vehicle collisions and traffic, property crime, domestic violence, sexual assault, shoplifting, purse snatching, and other crimes in commercial establishments, credit card fraud, money laundering, prostitution, local emergency services and other.

As stated, from the 2025 application, we did a literature review of studies that show the Encore Casino's impact on crime, and other studies that show gaming's influence on crime. This is the literature that we added into the 2025 application. If a summary of these reports is needed within this year's application, please let know.

• Assessing the Influence of Gambling on Public Safety in Massachusetts Cities and Towns Analysis of the influence of Encore Boston Harbor on its surrounding community during its first six months of operation Christopher W. Bruce Crime Analysis Consultant to the Massachusetts Gaming Commission

10 April 2020 v. 1.5

- CASINOS, CRIME, AND COMMUNITY COSTS Earl L. Grinols and David B. Mustard 2006
- Problem gambling and family violence: family member reports of prevalence, family impacts and family coping Aino Suomi, Alun C Jackson, Nicki A Dowling, Tiffany Lavis, Janet Patford, Shane A Thomas, Peter Harvey, Max Abbott, Maria E Bellringer, Jane Koziol-McLain and Sue Cockman

Boston Crime Incident Data 2024:

- In **District A1**, in 2024, the only Part One property crime category that saw an increase from 2023 was Other Larceny (1,342 to 1,402); while Part One violent crime was only up in the categories of Rape & Attempted and non-Domestic Aggravated Assaults (214 in 2023 to 249 in 2024).
- In **District A15**, in 2024, there was an increase in Auto Theft (19 in 2023 to 20 in 2024), Other Larceny (86 in 2023 to 125 in 2024) and Commercial Burglary (2 in 2023 to 6 in 2024) for Part One Property Crime categories. With regard to Part One Violent Crime, there were increases in Rape & Attempted, Robbery (12 in 2023 to 18 in 2024) and non Domestic Aggravated Assault (18 in 2023 to 28 in 2024).
- **Citywide:** Commercial Burglary and Other Larceny increased in Part One property crime for 2024; while only Aggravated Assaults (domestic and non-domestic) increased in Part one violent crime in 2024.

Boston Calls for Service Data 2024:

- In **District A1**, in 2024 DV and SA increased by +95, prostitution by +5 incidents, gang incidents decreased by -23 and overdose incidents increased by +30 from the 2023;
- In **District A15**, in 2024, there were decreases in DV/SA (-39), prostitution (no change), gang-related incidents (-7) and overdose (-21).
- Citywide, in 2024, DV/SA (+735), prostitution (+9) and gang related crime (+313) increased, while overdose incidents decreased by (-451).

MV Crash Data:

- **District A1,** total crashes in 2024 increased from 1029 to 987 in 2023, 152 with injuries compared to 140 in 2023, 1 fatal compared to 0 fatal in 2023, 84 pedestrian-involved compared to 80 in 2023 and 37 bicycle-involved versus 47 in 2023.
- **District A15,** total crashes decreased in 2024 with 262 compared to 371 in 2023; 50 with injuries compared to 46 in 2023; 1 fatal in 2024 and 1 fatal in 2023; 14 pedestrian-involved compared to 12 in 2023 and 17 bicycle-involved verse 11 in 2023.

Caveats: The 3 categories that we report here, i.e. calls for service, incident report and motor vehicle collision data, only include the 2 Districts that directly abut Encore Casino property. When reporting these abutting District data sets, we cannot assert a direct cause and effect to casino operations. Nor could we if we compared these District data sets to that of other COB Districts, that don't abut the properties. However, by analyzing this data, we can assume correlation, based on general increases in patterns of calls for service, incident reports and motor vehicle categories.

With regard to money laundering, sex trafficking, sexual assault, drug facilitated sexual assault, and gang activity related to casino sex trafficking, guns and drug sales; the crime increases in these categories are not significant within crime incident reporting data. Instead, the BPD has worked with other law enforcement agencies assigned to the casino on these crime categories, and over the past four years, there is evidence that these investigative activities are taking place.

Furthermore, as we noted in past applications, there are some crime categories that are difficult to quantify, as they are often not reported, thus do not show up via calls for service data, crime incident data or arrest data. Crimes most notable for underreporting include domestic violence, crimes against children, sexual assault, prostitution, gang-related crime activity (not a crime incident category, but instead a call for service data point) and overdose (also a call for service data point).

II. Please describe the project in detail and how the proposed project will address the impact indicated above. Please include a breakdown of the proposed scope of work, the scope should be sufficiently detailed to allow the review team to understand the steps required for project completion.

Scope of Work:

The BPD's Public Safety response will include strategies from 7 separate BPD internal Bureaus/Units/Offices. These internal sworn and civilian entities include:

- 1. The Office of the Police Commissioner (OPC)/ Office of Research and Development (ORD),
- 2. The Bureau of Investigative Services' (BIS) Family Justice Center's (FJC) Human Trafficking Unit (HTU),
- 3. BIS Drug Control Unit (DCU),
- 4. BIS Fugitive Apprehension Unit (FAU),
- 5. The Bureau of Field Services' (BFS) Youth Violence Strike Force (YVSF),
- 6. The BFS Area A1 & A15, and a new BPD Unit,
- 7. The BFS's Commercial Vehicle Enforcement Unit (CVEU)

1) The Office of the Police Commissioner (OPC) / Office of Research and Development (ORD)

The ORD is organizationally situated within the OPC, and is responsible for the research and development of innovative strategies and police practices that focus on issues relevant to modern policing. With that mandate, the ORD is responsible for the resource development of an estimated 13 million dollars per year; the administration, management and implementation of an estimated 28 million per year; and oversight of all research related projects that come to the BPD from internal and external partners.

The ORD is currently administering the implementation of (3) MGC CMF grant programs. Once the FY 2026 MGC CMF Block grant is funded; ORD will be administering the implementation of (4) MGC grant programs. This responsibility includes program implementation, financial management and reporting tasks. In addition, the ORD is interested in participating fully in all future MGC research projects, as we believe that we have a great deal to offer these research conversations, including; BPD crime data, organized and cleaned for quality use by MGC researchers, so that the MGC may gain the greatest benefit

from their research projects. Finally, ORD would like to help address one of the challenges that has come to ORD's attention while convening planning meetings for this application.

During the January 5th 2024 planning meeting, various BPD units met to discuss the grant application, and the needs presented as a result of Encore Casino operations. Commanders of these Units communicated the need for the BPD to coordinate and communicate better with all other law enforcement agencies working in, and around Encore Casino properties. Several Units communicated at the planning meeting, that they have tried to reach out to other law enforcement agencies involved in casino operations, including the MSP and casino security staff, with little to no communication back. Other Units had described a good relationship with those partners (i.e. the Human Trafficking Unit and Area A). During this meeting, Unit Commanders suggested that we form a partnership with all other law enforcement agencies serving casino properties, and meet with them consistently to share information and resources; especially given the varying types of crime activity that appear to be taking place as a result of casino operations, (i.e. money laundering, sex trafficking, drug dealing, domestic violence, sexual assault, gun related crime, etc.). Given this expressed need, as this partnership grows, the ORD will need support from the MGC to continue and expand this work, including:

- 1. Administering, implementing and financially managing the many strategies proposed in our current and future MGC grant programs;
- 2. Staffing of both internal and external law enforcement partnerships, collaborations and communication efforts ongoing through the development and staffing of an *Encore Casino Public Safety Task Force (Task Force)*;
- 3. Through that Task Force, facilitate the development of Memorandums of Understanding (MOU), amongst and between law enforcement agencies involved in casino operations; and
- 4. Provide to the MGC, the crime and research data needed for the MGC to effectively analyze the impact of gaming on Boston crime incidents in all Boston neighborhoods, while participating in any and all research partnerships embarked upon by the MGC.

Mitigation Strategy: In the 2025 Public Safety proposal, the MGC funded this position. Recently the BPD's Human Resources Department notified that the position will be posted within the month of February, and likely be hired by July 1, 2025.

The full time *Public Safety Program Coordinator* efforts will help to staff the MGC's future public-safety collaborative efforts, with all other law enforcement partners that serve the Encore Casino community. The entity by which this would happen would be an *Encore Casino Public Safety Task Force* – staffed by the *Public Safety Program Coordinator*. *Please see the attached DRAFT job description. This strategy would represent a mitigation strategy **not noted** in the RFP, but which may be effective in preventing and/or reducing crime incidents in and around the casino properties. In addition, this position will help with a mitigation strategy **noted** in the RFP- *Efforts to better track casino related crimes* - as the Program Coordinator will provide the crime data and analysis needed to track casino related crime.

Budget:

The BPD <u>will not need any funding for this position from July 1, 2025 through June 30th, 2026</u> as we currently have 2025 funding to cover that position from July 1, 2025 through June 30, 2026 at an MM6 Step 1 grade. However, given this 2026 grant will cover a 3-year time frame, we would like to utilize this year's grant to cover the two remaining years of July 1, 2026 – June 30, 2027 and July 1, 2027 through June 30, 2028. This

cost will include 2 years of salary (MM6 Step 2 & 3) and fringe (25.45%), and equal <u>\$175,33175</u>. Please see the attached excel spreadsheet for details on these costs.

ORD Subtotal: \$175,33175

2) The FJC's Human Trafficking Unit (HTU):

The BPD HTU is charged with three separate and distinct activities: 1) investigating criminal enterprises that profit from the sexual exploitation of vulnerable persons; 2) identifying and arresting buyers who drive the sex trade by increasing demand for the purchase of vulnerable persons; and 3) following up on referrals that come into the HTU by family members and service providers, of young persons that are missing, and assumed to be coerced, exploited and forced into Boston's sex trade. Follow-up activities are time-intensive and investigative in nature.

The sex trade industry in Boston, nationally and internationally is a multi-billion-dollar industry that has the financial means to constantly evolve and evade criminal prosecution through the use of new technologies, social media, normalization of sexual exploitation, online sale sites, transportation systems, untraceable monetary tools, and force and intimidation of victims. Human trafficking is the fastest-growing and second-largest criminal industry in the world, generating roughly \$150.2 billion worldwide. According to the *Trafficking in Persons Report* produced by the US State Department, approximately 27 million women, children and men are victims of some form of human trafficking; while 79% of human trafficking is sex trafficking.

Evidence that the operation of the Encore gaming facility has caused or is causing this impact is as follows:

The Human Trafficking Unit (HTU) has been working several investigations directly involved with Encore Casio for the past few years. These investigations are open & ongoing.

- The HTU is engaged in an investigation with the Massachusetts State Police High-Risk Victims Unit, the Massachusetts State Police Gaming Enforcement Unit, the Encore Anti-Laundering Unit, and Encore Security related to individuals believed to be involved in a variety of crimes based out of Roxbury and Dorchester. These individuals are believed to laundering their illicit gained funds through Encore where they frequent on a near daily basis.
- The HTU continues to move toward trial of an individual charged with **sex trafficking women**. The victims described being on the Encore Casino floor looking for dates on multiple dates before our alleged sex trafficker was arrested by the HTU in Boston. The detectives are currently traveling between Boston and Portland, Oregon prepping for this trial.
- The HTU is also assisting the Massachusetts State Police High-Risk Victims on an investigation of an individual who the Boston Police HTU has under active indictment for Human Trafficking and is suspected of selling designer club drugs at Encore and at bars in the Seaport area of Boston, in the furtherance of his Human Trafficking operation.

New cases: The Massachusetts State Police Gaming Unit and the Human Trafficking Unit continue to work on various investigations across Boston's neighborhoods with a nexus to the Encore Casino. We continue to target

demand in the Charlestown area by focusing on Encore patrons who purchase sex on websites. In October & November 2024, the Human Trafficking Unit along with our partners at the Massachusetts State Police High-Risk Victims Unit and the FBI Child Exploitation & Human Trafficking Task Force conducted operations. During the operation, 11 individuals were charged. Two of these individuals were charged with attempted rape of a child, enticement of a child, and solicitation of prostitution. These investigations are currently also open and ongoing. Just last week, the Human Trafficking Unit charged 2 individuals with Human Trafficking, both of these individuals were operating out of Encore and in a Boston neighborhood. We expect to charge several other individuals in the coming weeks related to these investigations.

The HTU continues and will continue to conduct demand reduction operations targeting sex-buyers that frequent the Encore Casino.

The HTU has also opened a Human Trafficking Drop-In Center in close proximity to the highway where investigators connect victims of human trafficking to basic need supplies and services. At the human trafficking drop-in center victims have the opportunity to shower, receive new clothes, and be connected with services to assist with their future.

Finally, as with all other 8 BPD Bureaus/ Units / Officers; the HTU will be required to attend all of the Encore Casino Public Safety Task Force Meetings. Given that, within this FY 2026 application, the HTU would like to request the following support items, through the City of Boston's (COB) block grant:

Overtime for HTU Detectives to work during hours that are not within their weekly tour of duty work hours, on joint law enforcement surveillance, demand reduction and other activities, to investigate and mitigate criminal activity on casino property, or related to casino activity in the crime categories of sex trafficking, money laundering and related drug trafficking. Estimated cost: **\$30,000** (\$30,000 / \$71.71 per hour = 418 hours)

Training for HTU Detectives: the BPD needs training in a wide range of areas to keep up with the evolving nature of the sex trade, and to understand the tools that they could use for harm reduction. Through this grant, the BPD's HTU will go to The <u>Juvenile Sex Trafficking (JuST) Conference</u> on October 22nd, 23rd and 24th 2025 in Phoenix, Arizona. The Juvenile Sex Trafficking (JuST) Conference was born in 2013 out of a vision to bring high-quality training to a national audience while simultaneously fostering collaboration at all levels of the community. This nationally-renowned conference features today's most pressing issues facing professionals and advocates in the anti-trafficking field. Workshops focus on skill-building, survivor experiences, cross-discipline collaboration, case studies, and lessons learned in the areas of investigation, prosecution, and therapeutic services. Each year offers new speakers and an array of powerful new workshops, with the goal of inspiring action and educating attendees on how to tackle child and youth sex trafficking from all sides. The BPD's HTU would like to send 5 HTU Detectives this year at a cost of **\$10,000**, which will include conference registration, round trip airfare and 2-night stay at a hotel in close proximity to the conference, ground transportation and per diem food expenses.

2 Dell XPS 17 Laptops are needed for HTU Detectives to utilize within their vehicles while conducting surveillance activities at the ENCORE, to do background checks and other investigative tasks. These 2 laptops will cost as estimated **\$4,400.00**

2024 Honda Odyssey: The HTU would like to purchase a 2024 Honda Odyssey to 1) transport victims of

casino sex trafficking cases to safe shelter locations statewide, so that they may feel safe while being interviewed and supported through each phase of investigation, prosecution and to start a new journey moving forward; and 2) to conduct surveillance while at Encore Casino properties. This vehicle will blend in with other casino vehicles, and not cause attention by presenting as a law enforcement vehicle. Estimated cost: **\$42,987.00**

HTU Subtotal: \$87, 387.00

3) BIS Drug Control Unit (DCU):

The DCU is dedicated to the enforcement of the controlled substance laws as they pertain to manufacturing and distribution. Our officers have established proven, legally sound methods of investigation to combat drug-related crimes from street level up to major trafficking levels to eradicate not only the dealers themselves, but also the various associated quality of life crimes such issues can present. The officers of the DCU are dedicated to combatting drug-related issues in every neighborhood in the COB, as well as working in collaboration with our neighboring law enforcement agencies to improve the quality of life for all residents.

The DCU officers are vastly experienced in investigating and preparing for the prosecution of criminals and drug gangs who perpetrate violence, fear and intimidation in our communities. These investigations often lead to not only the arrest and prosecution of those responsible for drug manufacturing and trafficking, but also the seizure of illicit drugs, illegal firearms and forfeiture of assets derived from drug traffickers at various levels in Boston and surrounding cities and towns. Boston DCU officers are extensively trained and highly skilled in undercover work, gathering surveillance, conducting drug buys, recruiting confidential informants as well as court preparation and testimony, and maintaining evidential and operational logs for long-range investigations. The DCU has coordinated and cooperated with state and local law enforcement officials on mutual drug enforcement efforts. With that said, the 70 officers assigned to DCU are at your disposal and available to work with the Encore Security team, Everett PD and Mass State Police in any capacity needed in a joint-efforts to combat drug activity at the Encore Casino.

The DCU has engaged many times in operations, jointly, with federal partners that have occurred, in part, at the Encore Casino. The officers assigned to the DCU, have acted in an undercover capacity, for the purpose of furthering investigations for crimes such as drug trafficking and money laundering. Some of these investigations are ongoing and giving details of these and other operations may jeopardize the integrity of the investigations and the officers. Through our participation in these investigations, we are aware that drug related crimes, and others, take place within the Encore Casino and its surrounding properties. Since 2021, money seized as proceeds from drug related crimes, has consistently decreased. According to the Department's Financial Evidence Office, in 2021, money seized by Boston officers totaled \$1,446,781.91; In 2022, it was \$948,129.75 and in 2023 the total was \$500,498.44. It can be reasonably inferred that there is a correlation of dwindling totals of seized drug money proceeds since the opening of the Encore Casino in 2019.

Although the BPD's DCU does not yet have a formal relationship with law enforcement LE agencies working in and around casino properties, they are very interested in meeting with those LE agencies and establishing a Memorandum of Understanding (MOU) with them to participate in multi-agency drug-related surveillance and crime investigation activities. The DCU, after establishment of an MOU, will utilize **Overtime (OT)** to participate in joint operations over the life of the grant. They will keep track of OT use, surveillance efforts, investigations, arrests and other related activities. In addition, officers of the DCU will actively participate in the *Encore Casino Law Enforcement Task Force* meetings and follow up strategies and investigations that come forth through increased collaboration and communication.

Impact: At this point in time, as the DCU has not yet operated in partnership with other LE partners on these activities, we assume that the impact will show in a decrease in drug-related crime incidents (i.e., overdose, drug sales, DFSA, illegal gun possession, etc.) on Encore properties, and the prevention of these crimes as it becomes apparent to drug dealers that LE is present and surveilling their activities.

Budget:

Overtime - \$6,000 / \$71.71 per hour = 84 hours

DCU Subtotal: \$6,000

4) BIS Fugitive Apprehension Unit (FAU);

The BDP FAU's primary function is to locate and apprehend individuals wanted for serious crimes. Requests for assistance in this matter come from a wide variety of sources both inside the BPD, and from partners both locally and federally. Fugitive Investigations include undercover surveillance both on foot and in unmarked vehicles. These activities are time intensive and investigative in nature. The Boston FAU currently works closely with the Massachusetts State Police Violent Fugitive Apprehension Section (VFAS), who works in and around the Encore Casino. The Boston FAU assists in surveillance and arrest of individuals wanted by the Massachusetts State Police (VFAS), and anticipates that we will assist in fugitive investigations in and around the Encore Casino in the future; as in the past, the Boston FAU has contacted the head of security for the Encore Casino, and the Massachusetts State Police Gaming Unit in furtherance of a fugitive investigations. The Boston FAU anticipates future Fugitive Unit investigations will include surveillance and apprehension in and around the Encore Casino of high-risk offenders.

Identified Impacts:

- **Increased visitation** and employment due to the casino will likely increase the interaction between public safety personnel and **casino patrons** and employees.
- It is recognized by law enforcement and the casino industry that certain types of crime may be attracted to casinos and other hospitality related businesses. These may include human trafficking, money laundering, and drug trafficking. Other crimes that may be attributable to casinos include increased assaults, fraud, and property crimes.

Mitigation Measures:

- **Collaborative** efforts to identify, monitor and address issues related to human trafficking, drug trafficking and money laundering.
- Collaborative efforts to better track casino related crimes

Strategy: The BPD FAU will participate in the *Encore Casino Public Safety Task Force* collaborative meetings and efforts, with both internal BPD Units and external law enforcement partners, to communicate more consistently about casino related and/or originated crime. And through that increased collaboration and communication, the FAU will be more involved in future joint law enforcement apprehension efforts - as these

operations will likely focus on known high-priority repeat offenders, who frequent casinos to commit crimes, **many of which may be fugitives of the law**.

Overtime for FAU Personnel: The FAU will need an estimated **\$6,000.00** in overtime to participate in internal and external planning meetings, surveillance and follow casino related investigations. \$6,000 / \$71.71 per hour = 84 hours

<u> Subtotal for FAU: \$6,000.00</u>

5) The Bureau of Field Services' (BFS) Youth Violence Strike Force (YVSF);

The YVSF's goal is to reduce the criminal activity and anti-social behavior of youthful offenders and youth gangs through directed and community-based policing strategies. The YVSF is very interested in meeting with those LE agencies that work with Encore and establishing a Memorandum of Understanding (MOU) with them to participate in multi-agency surveillance and crime investigation activities related to Boston-based gang affiliates. Anecdotally, it is believed that gang-related criminal activity, including drug sales, prostitution, and money laundering is taking place inside and around casino property. The YVSF, after establishment of an MOU, will utilize OT (\$5,000) to participate in joint operations over the life of the grant. We will keep track of OT use, surveillance efforts, investigations, arrests and other related activities.

The Youth Violence Strike Force which is commonly known as the Boston Police Gang Unit, is a multifaceted investigative and proactive unit, that is able to pull illegal firearms off the streets and stop violent acts from happening throughout the city and surrounding areas. At the same time, we are able to interact with the community through various inroads that we have made with the many stake holding groups in and around Boston.

Since 2021 this unit has arrested approximately 900 impact players driving the violence in and around the city. During that same time, we were able to pull 359 illegal firearms off the streets, a majority of which were carried by known gang members who were carrying these weapons to inflict harm. The various other arrests the Youth Violence Strike Force make are not for low level non-prosecutable misdemeanors, but rather high-level felonies where there is a victim on the other end who has been put in fear. We are also tasked by various agencies throughout the Commonwealth and beyond to assist them in a variety of ways including but not limited to Homicide investigation and apprehensions. We also work closely with the Mass State Police, US Marshalls, ATF and most of the surrounding municipalities throughout New England. We have also learned that there are various Boston Gangs laundering their illegal proceeds at Encore Casino as a way of cleaning their money.

Understanding the complexities of Policing in 2024 has enabled this unit to evolve into what it is today. We have established an unlimited amount of contacts within the community. Our purposeful engagement with each and every neighborhood in the city has allowed us to reach certain youth before they may be influenced by the bad actors who could steer them down the wrong path. Our partnerships include Ella J. Baker House and Youth Violence Reduction Task Force, Operation Exit, ROCA and Youth Connect, just to name a few. We realize that in order to build trust within the community we had to do more than make arrests. These partnerships have allowed us to be a partner with the community that we police, not a force of opposition.

Overtime for YVFS Personnel: The FAU will need an estimated **\$6,000.00** in overtime to participate in internal and external planning meetings, surveillance and follow casino related investigations. \$6,000 / \$71.71 per hour = 84 hours

Subtotal for YVSF: \$6,000.00

6) BFS Area A1 & A15:

Area A1 (Downtown Boston) and A15 (Charlestown) Districts have been negatively impacted by increased traffic flow in and around the roadways connected to the Encore Casino, including motor vehicle, pedestrian and bicycle collisions. In addition, there has been an increase in car thefts and motor vehicle larcenies. To combat the continuance of these incidents, the BPD needs high-visibility patrols in the area between 4:00 p.m. through 4:00 a.m., during high-traffic weekend days and evenings (Friday and Saturday). In addition, Detectives from these two Districts need additional time to implement strategies to combat the increase in car theft and larceny. These strategies could include undercover surveillance, community meetings to educate residents about target hardening tactics that protect their cars from theft and larceny, and additional hours to investigate car theft and larceny incidents. Overtime funds will be needed for all of these activities.

For this year's 2026 application, Area A, would also like to purchase **4 motorcycles and the uniforms and equipment** that are needed for a motorcycle patrol. The need for motorcycles to patrol the Encore properties and surrounding Area A jurisdiction has become more apparent as time has passed. With increased traffic congestion, and new construction projects coming on line each year, the BPD has realized the need for a quicker and more flexible response to traffic safety incidents, violations, collisions, heavy traffic, and traffic in poor weather; responses that can only be served by the speed and flexibility of a motorcycle patrol. Motorcycles are best suited for these roles because of their quick deployment and ability to conduct traffic enforcement in urban areas. Additionally, Motorcycles can be used to close streets and other traffic diversion functions or interest related to high profile guests. Motorcycles have a long and proven history as the best option for traffic enforcement in urban centers.

Identified Impacts that will be addressed:

- The presence of casinos has been demonstrated to cause an increase in cases of operating under the influence.
- Increases in traffic can cause increases in congestion, accidents, and vehicular/bicycle/pedestrian conflicts.

Mitigation Strategies that will be employed:

- Efforts to improve traffic safety that could include enhanced traffic enforcement, use of speed/message boards, public education programs, or other efforts that are demonstrated to improve traffic safety.
- Efforts to reduce impaired driving potentially including sobriety checkpoints, saturation patrols, education programs, or other demonstrated measures to reduce impaired driving.

Budget:

- Overtime for 2 dedicated traffic officers, 12 hours per day (4:00 pm 4:00 am), 2 days (F, S) a week (2 X 2 days X 12 hours X \$71.71 per hour = \$3,443.08 per week X 52 weeks per years = \$178,988.16 / \$71.71 per hour = 2,496 hours
- 2024 Harley -Davidson Motorcycles / FLHP Road King 4 X \$9,875.00 = \$119,956.00
- Gore-Tex Hi-Vis Super-shell Jacket 4 X \$519.99 = \$2,079.96
- Dark Navy Fleece Jackets \$110.96 X 4 = \$443.84
- Shipping \$12.00
- Frisk-master All-leather, Cut Resistant, Police Duty Glove 4 X \$48.95 = \$195.80
- Neos Adventure 15" Overshoes \$107.95 X 4 = \$431.80
- Shipping \$36.00
- Police Field Boots 4 X \$625.00 = \$2,500.00

Area A Subtotal: \$304,643.56

*As with all other BPD Bureaus/ Units / Officers; Area A supervisors will be required to attend all of the *Encore Casino Public Safety Task Force Meetings*.

8) The BFS's BSO Commercial Vehicle Enforcement Unit (CVEU) -

The Boston Police Department's Commercial Vehicle Enforcement Unit (Unit) is under the Bureau of Field Services and part of the Bureau of Special Operations. The Unit is composed of 3 officers and a Sergeant. Other than the Mass State Police Truck Team, the Unit is one of only 3 full time local law enforcement units solely dedicated to enforcing Federal Commercial Motor Vehicle Law in the State of Massachusetts. All officers assigned to the Unit have undergone approximately 3 years of specialized training and are Certified USDOT FMCSA Safety Inspectors. Through their certifications all members are authorized to conduct warrantless inspections on all property and passenger carrying commercial motor vehicles (CMV) on a public way. As per MGL Chapter 90, which deals with motor vehicle law, in order for a police officer to conduct a motor vehicle stop a violation must be observed. However, due to the extensive federal training and vetting process, the officers assigned to the Unit can conduct motor vehicle stops on these CMV's without observing a traffic violation. They are the only officers in the Boston Police Department that are authorized to do so. Additionally, the Unit is the only specialized group within BPD that are authorized to enforce Federal Motor Carrier Law, levy fines and confiscate those vehicles found to be unsafe or unfit for road travel.

Human traffickers often use commercial vehicles, including trucks, planes, trains and ships, to transport victims. [Human Trafficking Response Guide for the Transportation Industry, U.S. Department of Transportation] In fact, human trafficking can heavily depend on transportation systems in order to operate. For example, traffickers often use transportation hubs to recruit victims of sex trafficking and forced labor. Once recruited, traffickers may utilize transportation systems in order to relocate victims to other places where they will be abused and exploited.

The BPD's CVEU is in a unique position to recognize and report potential instances of human trafficking in the course of their work with commercial vehicles in the City of Boston. Unfortunately, none of the officers have been formally trained on how to detect signs of human trafficking. Furthermore, the unit lacks critical technology that would enable them to detect humans in the cargo holds of commercial vehicles. There are currently no efforts being made in this area. Therefore, the goal of this project is to expand the work of the BPD's Commercial Vehicle Enforcement Unit (CVEU) into a new area to better serve the Encore community, in and around the Encore properties.

Budget Request:

Training: Truckers Against Trafficking Training- <u>https://train.tatnonprofit.org/</u> September 21st through September 25th, 2025, Denver, CO – 3 BPD participants:

Registration: 750.00 X 3 = 2,250.00

Hotel Accommodations: 3 participants X 5 nights days X \$350.00 per night = \$5,250.00

RT Flights: 3 participants X 350.00 = 1,050.00

\$8,550.00

Overtime for additional truck stops and inspections, OT and OT for backfill for participating in training. Overtime: \$82.00 x 7.5 hrs. = \$615 PER SHIFT, \$615 x 3 members= \$1,845 A WEEK, \$1,845 X 52 weeks = \$95,940

CVEU Subtotal: \$104,490.00

Public Safety Strategy Total: \$758,837.54

Proposed MGC Grant Budget

Please use the following table to outline the project budget. Please include as an attachment any requests for proposals, quotes, or estimates that would quantify the costs associated with the mitigation.

Description of Purchase/Work	Timeline	QTY	Budget
FT Program Coordinator Salary & Fringe - 24 months	July 1, 2026 - June 30, 2028	1	175,331.75
Overtime for 7 Bureaus, Units and Officers	Jan. 2026 - June 30, 2028	7	322,928.16
Human Trafficking Training for the HTU	October 2025	5	10,000.00
Truckers Against Trafficking Training for the CVEU	September 2025	3	8,550.00
Dell XPS 17 Laptop	Jan 2026 - June 2028	2	4,400.00
Gore-tex Hi-Vis Super Shell Jackets for Area A MOPS	Jan 2026 - June 2028	4	2,079.96
Dark Navy Fleece Jackets for Area A MOPS	Jan 2026 - June 2028	4	443.84
Frisk-master all leather police gloves	Jan 2026 - June 2028	4	195.80
Neos Adventure Overshoes	Jan 2026 - June 2028	4	431.80
Police Field Boots	Jan 2026 - June 2028	4	2,500.00
Shipping for all police Motorcycle Uniform supplies and equipment	Jan 2026 - June 2028	4	48.00
Vehicle the HTU - 2024 Honda Odyssey	Jan 2026 - June 2028	1	42,987.00
Harley-Davidson Police Motorcycles for Area	Jan 2026 - June 2028	4	119,956.00

А		
Subtotal		689,852.31
10% City of Boston Admin. Rate		68,985.23
TOTAL		758,837.54

*Please see in the attachments section a more detailed Budget & Timeline

Part D- Gambling Harm Reduction- The application should include sufficient backup information for the review team to fully understand the project(s). This information could include other relevant research, Requests for Proposals, etc.

Project Name:				
Please provide below the contact	contact information for the individual managing this aspect of the gra Additional Project Contact (if applicable) Name: Titlo:			
Project Contact:	Additional Project Contact (if applicable)			
Name:	Name:			
Title:	Title:			
Department:	Department:			
Email Address:	Email Address:			
Telephone:	Telephone:			
Address:	Address:			

I. Please use the space below to identify the impact of the gaming establishment on your municipality. You may use the impacts identified in the FY 2026 Guidelines relevant to this category. If you are using an impact not identified in the guidelines, please use the space below to identify the impact. Please provide documentation or evidence that gives support for the determination that the operation of the gaming facility caused or is causing the impact (i.e., surveys, data, reports, etc.)

II. Please describe the project in detail and how the proposed project will address the impact indicated above. Please include a breakdown of the proposed scope of work, the scope should be sufficiently detailed to allow the review team to understand the steps required for project completion.

Proposed MGC Grant Budget

Please use the following table to outline the project budget. Please include as an attachment any requests for proposals, quotes, or estimates that would quantify the costs associated with the mitigation.

Description of Purchase/Work	Timeline	QTY	Budget
	TOTAL:		

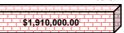
Part E- Specific Impact If you are applying for a SI grant you should reach out in advance to MGC Staff to ensure project eligibility. A community may also use this Specific Impact Grant to break out administrative and staffing costs associated with the grant as a whole. -

out administrative and staffing costs assoc	iated with the grant a	is a whole	
Project Name:			
Please provide below the contact information	tion for the individual	managing th	is aspect of the
grant.			
Project Contact:	Additional Pr	roject Contac	t (if applicable)
Name:	Name:		
Title:	Title:		
Department:	Department:		
Email Address:	Email Addres	s:	
Telephone:	Telephone:		
Address:	Address:		
 Please use the space below to iden municipality. This category is for pr any impacts identified in the FY 202 not identified in the guidelines, ple provide documentation or evidenc operation of the gaming facility cau etc.) 	ojects that do not fit 26 Guidelines that are ase use the space bel e that gives support f	in any other of e relevant. If y low to identify or the determ	category but may use you are using an impact y the impact. Please hination that the
II. Please describe the project in detail impact indicated above. Please incl scope should be sufficiently detaile required for project completion.	lude a breakdown of t	the proposed	scope of work, the
Proposed	MGC Grant Budget		
Please use the following table to outline requests for proposals, quotes, or estima mitigation.			
Description of Purchase/Work	Timeline	QTY	Budget

COST ESTIMATE SUMMARY

PS&E Estimates





36% Co

\$748,000.00 Date: January 14, 2025 Project No:

ITEM #	DESCRIPTION INDIVIDUAL TREE PROTECTION	UNIT	UNIT PRICE \$ 750.00	Chelsea & Terminal	Total	COST
02.51 02.52 02.53	TEMPORARY TREE PROTECTION TEMPORARY TREE PROTECTION FENCE TREE PRUNING	FT EA	\$ 750.00 \$ 37.50 \$ 412.50		0	s s s
07.855* 19.101	PRESSURE INJECTION OF CRACKS RODENT CONTROL HIGH R-FACTOR (LOCATION #1)	LF LOCATION	\$ 600.00 \$ 30,000.00	1	1	\$ 600 \$ 30,000
19.102 19.103 19.104	RODENT CONTROL HIGH R-FACTOR (LOCATION #02) RODENT CONTROL HIGH R-FACTOR (LOCATION #03) RODENT CONTROL HIGH R-FACTOR (LOCATION #04)	LOCATION LOCATION	\$ 30,000.00 \$ 30,000.00 \$ 30,000.00		0	\$ \$ \$
20.1	UNCLASSIFIED EXCAVATION CLASS A ROCK EXCAVATION	CY	\$ 75.00 \$ 75.00	765 10	765 10	\$ 57,375 \$ 750
29.1 44.	HOT MIX ASPHALT MILLING 0*-6* CLASS B ROCK EXCAVATION	SY CY	\$ 50.00 \$ 75.00	130 10	130 10	\$ 6,500 \$ 750
15. 16. 51.02	DRAINAGE STRUCTURE ABANDONED DRAINAGE STRUCTURE REMOVED GRAVEL	EA EA TON	\$ 1,500.00 \$ 2,500.00 \$ 55.00	1,370	0 0 1370	\$ \$ 75,350
53.5 54.01	CONTROLLED DENSITY FILL FLOWABLE & EXCAVATABLE SAND FOR CONDUIT TRENCH	CY TON	\$ 450.00 \$ 50.00	13	0 13	\$ 650
56.12 91.	CRUSHED STONE FOR CURB FOUNDATION DRIVE SAMPLE BORING	TON FT	\$ 55.00 \$ 250.00	50	50 0	\$ 2,722
91.10 91.11 91.61	HOLLOW STEM AUGER BORINGS CORE BORING TEST PIT ECAVATION	FT FT CY	\$ 250.00 \$ 250.00 \$ 150.00	15	0 0 15	\$ \$ \$ 2.250
91.9* 93.	SOIL BORINGS AT SIGNAL MASTARM FOUNDATIONS MOBILIZATION AND DISMANTLING OF BORING EQUIPMENT	RD	\$ 10,000.00 \$ 3,125.00	1	1 0	\$ 10,000 \$
20.5	DRAINAGE STRUCTURE REMODELED DENSE GRADED CRUSHED STONE FOR SUB-BASE	EA TON	\$ 2,000.00 \$ 50.00	285	0 285	\$ \$ 14,250
31.1 40. 43.	HIGH EARLY STRENGTH CEMENT CONCRETE BASE COURSE CALCIUM CHLORIDE FOR ROADWAY DUST CONTROL WATER FOR ROADWAY DUST CONTROL	CY LB MGAL	\$ 250.00 \$ 2.50 \$ 100.00	10 4,000 4	10 4000 4	\$ 2,500 \$ 10,000 \$ 400
50.45 51.	FIBER SEALING CRACKS HMA FOR PATCHING	GAL	\$ 175.00 \$ 500.00	5	5	\$ 875 \$
55.235 55.325	SUPERPAVE SURFACE COURSE - 12.5 (SSC - 12.5 MM) SUPERPAVE INTERMEDIATE COURSE - 19.0 (SIC - 19.0 MM)	TON	\$ 350.00 \$ 350.00	240 605	240 605	\$ 84,000 \$ 211,750
63.5 72. 72.5	BITUMINOUS TACK COAT HOT MIX ASPHALT FOR MISCELLANEOUS WORK UTILITY TRENCH REPAIR	GAL TON SY	\$ 25.00 \$ 750.00 \$ 100.00	310 20 20	310 20 20	\$ 7,750 \$ 14,625 \$ 2,000
82.4 82.41*	SAW CUT SAWCUTTING PORTLAND CEMENT CONCRETE FOR WALL REPAIR	LF	\$ 25.00 \$ 50.00	150 40	150 40	\$ 3,750 \$ 2,000
04. 04.1	GRANITE CURB TYPE VA4 - STRAIGHT GRANITE CURB TYPE VA4 - CURVED	LF	\$ 85.00 \$ 125.00	150 150	150 150	\$ 12,750 \$ 18,750
04.2 07.	GRANITE CURB TYPE VA4 - SPLAYED END TRAVERSABLE GRANITE CURBING, TYPE T-100 STRAIGHT TRAVERSABLE GRANITE CURBING, TYPE T-100 CURVED	LF	\$ 106.25 \$ 150.00 \$ 200.00	10	0	\$ \$ 1,500
07.1 08.6* 08.7*	IRAVERSABLE GRANTE CURBING, TYPE T-100 CURVED GRANITE TRANSITION CURB TYPE T-100 TO TYPE VA4 (STRAIGHT) GRANITE TRANSITION CURB TYPE T-100 TO TYPE VA4 (CURVED)	LF EA EA	\$ 200.00 \$ 200.00 \$ 250.00	30 1 1	30 1 1	\$ 6,000 \$ 200 \$ 250
14. 16.	GRANITE CURB INLET - STRAIGHT GRANITE CURB CORNER TYPE A	EA EA	\$ 850.00 \$ 500.00	1	1	\$ 850 \$
10. 10.	CURB REMOVED AND RESET CURB REMOVED AND STACKED	LF FT	\$ 75.00 \$ 43.75	400	400 0	\$ 30,000 \$
32.4 20.1 27.82*	INDIVIDUAL POST REMOVED AND DISCARDED STEEL BEAM HIGHWAY GUARD (SINGLE FACED) GUARDRAIL TANGENT END TREATMENT, TL-2	EA LF	\$ 50.00 \$ 400.00 \$ 4,400.00	380	0 380 4	\$ \$ 152,000 \$ 17,600
27.82* 28.24* 22.*	GUARDRAIL TANGENT END TREATMENT, TL-2 TRANSITION TO BRIDGE RAIL VEHICULAR DOUBLE SWING GATE	EA EA LS	\$ 6,500.00 \$ 35.000.00	4	4 1 1	\$ 17,600 \$ 6,500 \$ 35,000
97.1* 01.	CATCH BASIN INSERT FOR EROSION CONTROL CEMENT CONCRETE SIDEWALK	EA	\$ 150.00 \$ 325.00	7 140	7 140	\$ 1,050 \$ 45,500
)1.1)1.2	CEMENT CONCRETE SIDEWALK AT DRIVEWAYS CEMENT CONCRETE PEDESTRIAN RAMP HOT MIX ASHPHALT WALK SURFACE	SY SY	\$ 325.00 \$ 425.00	20	0 20	\$ 8,500
)2.)3.)7.16	HOT MIX ASHPHALT WALK SURFACE HOT MIX ASPHALT DRIVEWAY PLANTERS REMOVED AND RESET	TON TON EA	\$ 350.00 \$ 350.00 \$ 125.00	20	0 20 0	\$ \$ 7,000
11.	BOUND REMOVED AND RESET CITY COVERS ADJUSTED	EA	\$ 125.00 \$ 500.00 \$ 450.00	1	1	\$ 500 \$
51.5 67.6	LOAM AND SEEDING AGED PINE BARK MULCH	CY CY	\$ 200.00 \$ 100.00	45	45	\$ 9,000 \$
69.* 04.22	PAVEMENT MILLING MULCH UNDER GUARD RAIL 2-INCH ELECTRICAL CONDUIT, TYPE NM, PLASTIC (UL) (MS) 3-INCH RIGID PLASTIC CONDUIT FOR TRAFFIC SIGNAL	LF	\$ 15.00 \$ 95.00	420	420	\$ 6,300 \$ 950
04.301 04.33 04.401	3-INCH RIGID FLASTIC CONDUIT FOR TRAFFIC SIGNAL 3-INCH ELECTRICAL CONDUIT, TYPE IMI, PLASTIC (UL) (MS) 4-INCH RIGID PLASTIC CONDUIT FOR TRAFFIC SIGNALS	LF LF FT	\$ 110.00 \$ 110.00 \$ 150.00	210 260	210 260 0	\$ 23,100 \$ 28,600 \$
11.301	TRAFFIC HANDHOLE, 12" X 24" 12-INCH X 24-INCH HANDHOLE WITH FRAME AND COVER	EA	\$ 2,750.00 \$ 2,750.00	3	3	\$ 8,250 \$
11.331 11.332	PULL BOX_PRECAST 12" X 24" (MUNICIPAL STANDARD) PULLBOX PRE-CAST SHORT 12 X 24 (MUNICIPAL STANDARD)	EA EA	\$ 2,750.00 \$ 3,500.00	2	2	\$ 5,500 \$
11.341	PRE-CAST CONCRETE PULLBOX EXTENDER – 6" (MS) PRE-CAST CONCRETE PULLBOX EXTENDER – 3" (MS)	EA	\$ 750.00 \$ 750.00		0	\$
11.351 11.363 11.391	PULL BOX ADJUSTED PULL BOX FRAME AND COVER (SIDEWALK) REMOVE AND REPLACE PULLBOX REMOVED AND STACKED	EA EA EA	\$ 500.00 \$ 1,000.00 \$ 625.00	1	1	\$ 1,000 \$ 1,000 \$
12.092	LIGHT STANDARD BASE-PRE-CAST (MUNICIPAL STANDARD) LIGHT STANDARD BASE-PRE-CAST SHORT BASE (MUNICIPAL STANDARD)	EA	\$ 3,000.00 \$ 3,000.00	1	1	\$ 3,000 \$
2.094	LIGHT STANDARD BASE-PRE-CAST SHORT MODIFIED BASE (MUNICIPAL STANDARD) LIGHT STANDARD BASE-PRE-CAST (MUNICIPAL STANDARD) IN SOD AREA LIGHT STANDARD BASE-PRE-CAST SHORT BASE (MS) IN SOD AREA	EA	\$ 3,000.00 \$ 3,500.00	2	0	\$ 7,000
12.159 12.160 12.201	LIGHT STANDARD BASE-PRE-CAST SHORT BASE (MS) IN SOD AREA LIGHT STANDARD BASE-PRE-CAST SHORT MODIFIED BASE (MS) IN SOD AREA CONTROL CABINET BASE - PRE-CAST (MUNICIPAL STANDARD)	EA EA EA	\$ 3,500.00 \$ 3,500.00 \$ 1,500.00		0	s s
12.201 12.211 12.301	EVERSOURCE MANULE BREAK FOR LIGHTING CONTROLLER TRAFFIC SIGNAL POST FOUNDATION	EA	\$ 12,500.00 \$ 12,500.00 \$ 1,500.00	4	0 4	\$ \$ 6,000
12.411	TYPE II MASTARM POLE FOUNDATION TRAFFIC SIGNAL CONTROL CABINET FOUNDATION GROUND MOUNT (FOR	EA	\$ 17,000.00 \$ 5.000.00	1	i	\$ 17,000
13.40	TYPE CB, CC CABINET) WIRE TYPE 8 No.10 DIRECT BURIAL WIRE TYPE 8 No.4 DIRECT BURIAL	LF	\$ 1.95	40	1 40	\$ 5,000 \$ 78
13.431 13.650 13.781	WIRE TYPE 8 No. 4 DIRECT BURIAL STREET LIGHTING NONCONDUCTIVE INSULATING SLEEVE (MS) INTERCONNECT CABLE SYSTEM (12 STRAND SINGLE MODE FIBER OPTIC CABLE)	EA FT	\$ 7.00 \$ 150.00 \$ 12.50	900 2	900 2 0	\$ 6,300 \$ 300 \$
16.01* 16.02	TRAFFIC SIGNAL LOCATION NO.1 TRAFFIC SIGNAL LOCATION NO.1	LS	\$ 120,000.00 \$ 331,250.00	1	1	\$ 120,000 \$
16.03 16.04	TRAFFIC SIGNAL RECONSTRUCTION LOCATION NO. 3 TRAFFIC SIGNAL RECONSTRUCTION LOCATION NO. 4	LS	\$ 160,000.00 \$ 162,500.00		0	s
16.05 20.151	TRAFFIC SIGNAL RECONSTRUCTION LOCATION NO. 5 TEMPORARY LIGHTING	LS	\$ 41,250.00 \$ 25,000.00		0	\$
23.24 23.71 23.72	REMOVE AND RESET LANDSCAPE LIGHTING STREET LIGHTING POST & LUMINAIRE REMOVE & RELOCATE STREET LIGHTING POST AND LUMINAIRE REMOVE AND TRANSPORT	EA EA EA	\$ 625.00 \$ 4,500.00 \$ 1.500.00	3	0 3 0	\$ \$ 13,500 \$
26.53 32.1	STREET LIGHTING POST AND LUMINAIRE REMOVE AND TRANSPORT FIRE ALARM BASE AND MIDDLE SECTION ADJUSTED WARNING-REGULATORY AND ROUTE MARKER-ALUMINUM PANEL (TYPE A)	EA	\$ 2,500.00 \$ 2,500.00 \$ 20.00	15	0	\$ \$ 300
34.18 34.187	TRAFFIC DELINEATOR POST (WHITE) WITH MODULAR BASE MODULAR RASIED CURB WITH 36" DELINEATOR POST WHITE OR YELLOW	EA EA	\$ 175.00 \$ 350.00		0	s s
52. 52.12	SAFETY SIGNING FOR CONSTRUCTION OPERATIONS TOW ZONE NO PARKING SIGNS ALUMINUM PANEL TYPE A	SF EA	\$ 25.00 \$ 25.00	300 6	300 6	\$ 7,500 \$ 150
53.11 53.3 54.014	TEMPORARY PRE-CAST CONCRETE MEDIAN BARRIER PORTABLE BREAKAWAY BARRICADE TYPE III TEMPORARY PAVEMENT MARKINGS – 4-INCH (PAINTED)	LF EA LF	\$ 80.00 \$ 300.00 \$ 2.00	100 2 1000	100 2 1000	\$ 8,000 \$ 600 \$ 2,000
54.014 54.2 56.	PAVEMENT MARKING REMOVAL ARROW BOARD	SF DAY	\$ 5.50 \$ 15.00	1000 60	1000 60 0	\$ 2,000 \$ 330 \$
56.12* 59.	PORTABLE CHANGEABLE MESSAGE SIGN REFLECTORIZED DRUM	DAY DD	\$ 150.00 \$ 0.75	30 3600	30 3600	\$ 4,500 \$ 2,700
59.1 54.031 54.032	REFLECTORIZED DRUM WITH FLASHER (TYPE A) PAVEMENT LEGEND REFL. WHITE THERMOPLASTIC (PREFORMED) - BIKE LANE RIDER	DD EA EA	\$ 0.75 \$ 625.00 \$ 750.00	1200	1200 0 0	\$ 900 \$ \$
54.032 54.035 54.037	PAVEMENT LEGEND REFL. WHITE THERMOPLASTIC (PREFORMED) - BIKE LANE ARROW PAVEMENT LEGEND REFL. WHITE THERMOPLASTIC (PREFORMED) - SHARROWS PAVEMENT LEGEND REFL. WHITE THERMOPLASTIC (PREFORMED) - RAISED CROSSWALK	EA EA	\$ 750.00 \$ 750.00 \$ 562.50		0	s s
54.04 54.08	PAVEMENT ARROWS AND LEGENDS REFL. WHITE (THERMOPLASTIC) COLORED COATING (GREEN) FOR BIKE LANE	SF SF	\$ 10.50 \$ 20.00		0	\$ S
35.1 36.104	CROSSWALKS AND STOP LINES REFLECTORIZED WHITE (THERMOPLASTIC 4 INCH REFLECTORIZED WHITE LINE (THERMOPLASTIC)	LF LF	\$ 7.50 \$ 1.75	250 170	250 170	\$ 1,875 \$ 297
66.106 66.108 66.112	6 INCH REFLECTORIZED WHITE LINE (THERMOPLASTIC) 8 INCH REFLECTORIZED WHITE LINE (THERMOPLASTIC) 12 INCH REFLECTORIZED WHITE LINE (THERMOPLASTIC)	FT FT FT	\$ 1.75 \$ 1.75 \$ 7.50		0	\$ \$
57.104 57.108	4 INCH REFLECTORIZED YELLOW LINE (THERMOPLASTIC) 8 INCH REFLECTORIZED YELLOW LINE (THERMOPLASTIC)	LF	\$ 2.00 \$ 2.00	985 115	985 115	\$ 1,970 \$ 230
57.112 74.09	12 INCH REFLECTORIZED YELLOW LINE (THERMOPLASTIC) STREET NAME SIGN 9-INCH	FT	\$ 8.00 \$ 150.00	1	0	\$ \$ 150
4.018 4.2 4.3	STREET NAME SIGN 18-INCH ON MAST ARM TRAFFIC SIGN REMOVED AND RESET TRAFFIC SIGNS REMOVED AND STACKED	EA EA EA	\$ 1,000.00 \$ 250.00 \$ 50.00	1	1 2 0	\$ 1,000 \$ 500 \$
74.3 77.2 77.3	TRAFFIC SIGNS REMOVED AND STACKED SIGN POST REMOVED AND STACKED SIGN POST 2 INCH STEEL	EA EA	\$ 50.00 \$ 48.00 \$ 175.00	2	0	\$ \$ 350
14.72 15.2	MISCELLANEOUS CLASS D CEMENT CONCRETE MASONRY 5000 PSI, 3/8 INCH, 710 CEMENT CONCRETE	CY CY	\$ 500.00 \$ 8,000.00	5	5	\$ 2,500 \$ 8,000
19.09 19.122*	STEEL SIDEWALK PLATE FOR UTILITIES CONCRETE REPAIR - DEEP	EA SF	\$ 2,500.00 \$ 250.00	20	0 20	\$ \$ 5,000
10.1 75.2* 1-2	STEEL REINFORCEMENT FOR STRUCTURES - EPOXY COATED METAL BRIDGE RAILING (3 RAIL), ALUMINUM (TYPE AL-3) BELOW GRADE EXCAVATION	LB LF CY	\$ 10.00 \$ 500.00 \$ 163.00	260 105	260 105 0	\$ 2,600 \$ 52,500 \$
1-2 1-3 2-1	BELOW GRADE EXCAVATION ROCK EXCAVATION GRAVEL	CY CY CY	\$ 163.00 \$ 250.00 \$ 50.00		0	\$ \$
3-1 7-1	SCREENED GRAVEL SHEATHING INSTALLED	CY SF	\$ 75.00 \$ 31.25		0	\$
1-6 3-1	LAY 6 INCH DICL WATER PIPE SET OR RESET HYDRANT	FT EA	\$ 750.00 \$ 5,500.00	1	0	\$ \$ 5,500
9-1A 1-04	ADJUST WATER CASTINGS TO GRADE CHIMNEY CONNECTIONS	EA VF	\$ 250.00 \$ 625.00 \$ 325.00	5	5	\$ 1,250 \$
1-06 2-01 2-02	MINOR DRAIN MANHOLE BASE MANHOLE WALLS	FT EA VF	\$ 325.00 \$ 15,500.00 \$ 200.00		0	s s
2-02 2-03 2-3A	MANHOLE FRAME WITH COVER TRANSPORT OF FRAME AND GRATE OR COVER	EA	\$ 1,500.00 \$ 500.00	2	0	\$ \$ 1,000
2-4	CATCH BASIN	EA	\$ 15,500.00		ō	\$

	Project: Mystic River Harborwalk	Computed:	C.Al	lison	Date	: 11/6/20	24	
1.57	Project ID: MyRWA	Checked:	A.Be	aulac	Date	e: 1/22/20	25	
D	HDR Job #: 10367504	Page:	1	1	0	f:		1
	SUBMISSION NAME							
Item No.	Item Description		Unit	Quantity		Unit Price		Cost
101.1	CLEARING AND GRUBBING		SY	1,750	\$	11.00	\$	19,250.00
120.	EARTH EXCAVATION		CY	5,900	\$	55.00	\$	324,500.00
127.	CONCRETE EXCAVATION		CY	74	\$	225.00	\$	16,650.00
156.12	CRUSHED STONE FOR CURB FOUNDATION		TON	28	\$	60.00	\$	1,680.00
156.8	CRUSHED STONE FOR SUB-BASE		CY	600	\$	45.00	\$	27,000.00
170.	FINE GRADING AND COMPACTING - SUBGRADE AREA		SY	1,200	\$	15.00	\$	18,000.00
181.11	DISPOSAL OF UNREGULATED SOIL		TON	2,950	\$	50.00	\$	147,500.00
201.	CATCH BASIN		EA	6.73	\$	9,000.00	\$	60,563.08
203.01	SPECIAL MANHOLE (WQU#1)		EA	1	\$	10,500.00	\$	10,500.00
203.02	SPECIAL MANHOLE (WQU#2)		EA	1	\$	10,500.00	\$	10,500.00
203.03	SPECIAL MANHOLE (WQU#3)		EA	1	\$	10,500.00	\$	10,500.00
203.04	SPECIAL MANHOLE (WQU#4)		EA	1	\$	13,000.00	\$	13,000.00
224.12	12 INCH HOOD		EA	6	\$	350.00	\$	2,100.00
241.12	12 INCH REINFORCED CONCRETE PIPE CLASS III		LF	303	\$	140.00	\$	42,420.00
451.	HMA FOR PATCHING		TON	50	\$	290.00	\$	14,500.00
504.	GRANITE CURB TYPE VA4-STRAIGHT		LF	990	\$	80.00	\$	79,200.00
630.	HIGHWAY GUARD REMOVED AND RESET		LF	100	\$	35.00	\$	3,500.00
635.1	HIGHWAY GUARD REMOVED AND DISCARDED		LF	137.5	\$	8.00	\$	1,100.00
655.2	ORNAMENTAL HAND RAIL		LF	904	\$	150.00	\$	135,600.00
665.	CHAIN LINK FENCE REMOVED AND STACKED		LF	1,020	\$	8.00	\$	8,160.00
697.	SEDIMENTATION FENCE		LF	100	\$	12.00	\$	1,200.00
697.1	SILT SACK		EA	4	\$	250.00	\$	1,000.00
697.2	FLOATING SILT FENCE		LF	1,050	\$	30.00	\$	31,500.00
698.3	GEOTEXTILE FABRIC FOR SEPARATION		SY	1,600.00	\$	5.66	\$	9,056.00
702.	HOT MIX ASPHALT SIDEWALK OR DRIVEWAY		TON	21	\$	300.00	\$	6,300.00
748.	MOBILIZATION		LS	1	\$	118,922.96	\$	118,922.96
754.	LOAM AND SEEDING		CY	160	\$	100.00	\$	16,000.00
823.71	HIGHWAY LIGHTING POLE AND LUMINAIRE REMOVED AND STACKED		EA	5	\$	20.000.00	\$	100,000.00
901.	4,000 PSI 1.5-INCH, 565 CEMENT CONCRETE		CY	589.62	\$	1,725.00		1,017,091.94
910.	STEEL REINFORCEMENT FOR STRUCTURES		LB	103,200	\$	4.00	\$	412.800.00
X3			LF	1,012.00	\$	130.00	Ŧ	131,560.00
X4	UTILITY RELOCATIONS		LS	1.00	\$	5.000.00		5,000.00
X7	CHINKING STONE WALL		SF	3.050.38	\$	75.00	\$	228.778.50
Amenities			, <u> </u>	2,000.00	I Ť		Ť	
	SURFACE MATERIALS (PAVING)		LS	1	\$	39,793.00	\$	39,793.00
	WALLS & CURBS		LS	1		111,128.00	\$	111,128.00
	SITE ELEMENTS: GENERAL		LS	1		743,607.00	\$	743,607.00
	SITE ELEMENTS: EXERCISE & PLAY		LS	1	\$	19,418.00	\$	19,418.00
	PLANTING		LS	1		114,440.00	Ф \$	114,440.00
	PLANTING SOILS		LS	1	\$	29,203.00		29,203.00
	PLANTING SUILS		LS	1	\$	29,203.00	\$	29,20

Subtotal: \$4,083,021.48

- General Conditions
 10%
 \$
 408,302.15

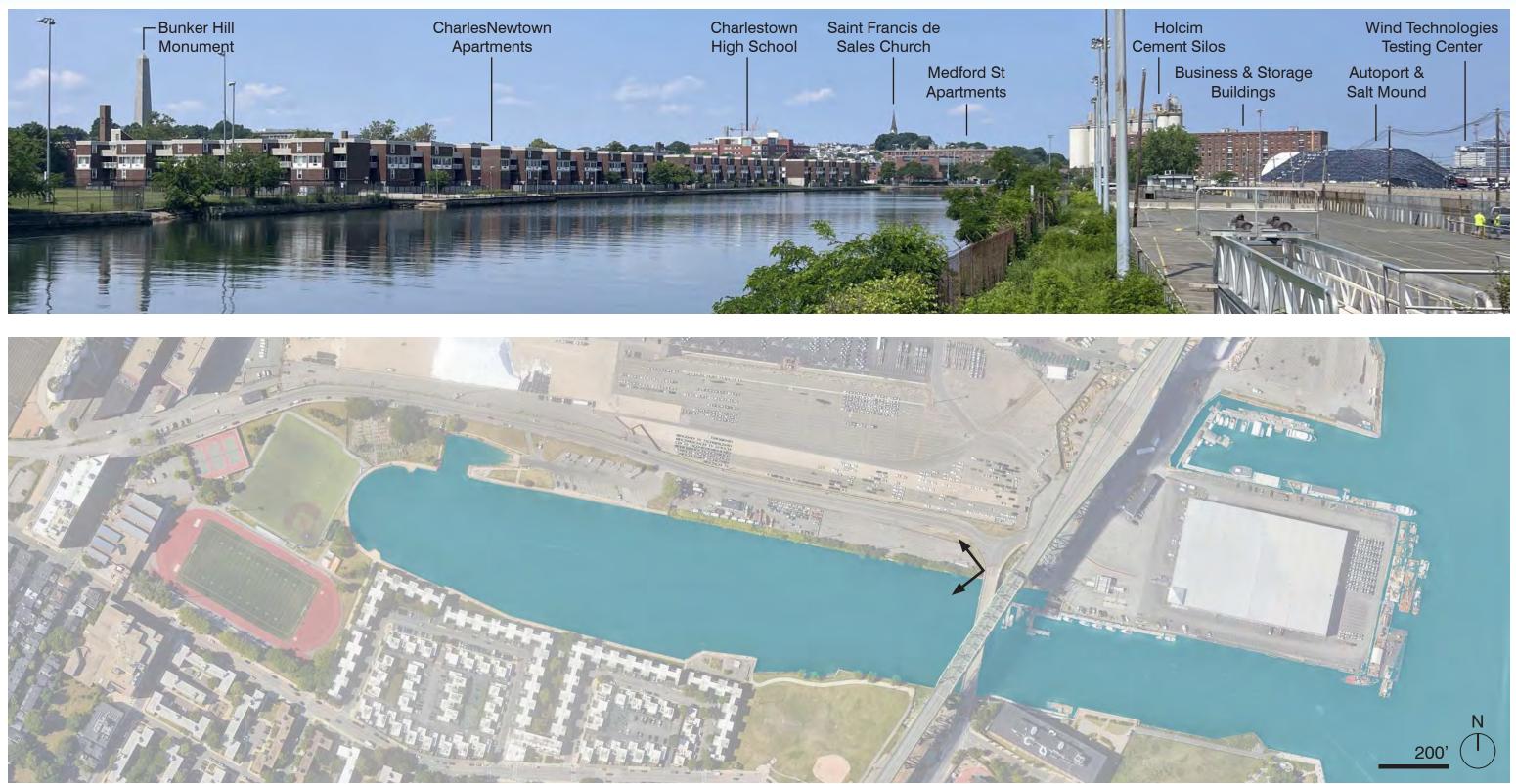
 Contingency
 20%
 \$
 816,604.30
 - Construction Item Total: \$ 5,307,927.92
- Construction Engineering (8%): \$ 424,634.23
 - **PROJECT TOTAL:** \$ 5,732,562.15

LOW PROJECT TOTAL (-10%): \$ 5,159,305.94 HIGH PROJECT TOTAL (+25%): \$ 7,165,702.69

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Task	Notes	April 20	024 N	May 202	4 Ju	ne 2024	l Jul	y 2024	Aug	2024	Sept 2	024	Oct 202	24 N	lov 202	24 D	ec 202	24 J	an 202	5 F	eb 202	5 Ma	rch 2025	Apri	2025	Ma	y 2025	June	2025	July 2	2025	Aug	2025	Sept	2025	Oct	2025	Nov 2)25	Dec	2025	Jan 3	2026	Feb 2	2026	March	2026	April 2	2026	May 2	J26
Landscape Design																																																		\square	
Submittal/Review of MEPA Letter	Letter submitted 4/4/2024																																																	\square	L
Drainage Analysis																																																			
Drainage Report																																																			
Structural Design of Wall (75%)																																																		ΠT	Т
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75% Plans Submittal Review																						T																										T		\square	Τ
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Preparation of SV Form (USAC Perm	it)																																																	\square	Τ
USACE Review	30 day comment period																																																	\square	Τ
Structural Design of Wall (100%)																																																		\square	Т
100% Plans Submittal Preparation																													5																			T		\square	Τ
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Little Mystic Channel Harborwalk Extension **SITE CONDITIONS - VIEWS**







Little Mystic Channel Harborwalk Extension **SITE CONDITIONS -** VIEWS







Little Mystic Channel Harborwalk Extension SITE CONDITIONS - VIEWS



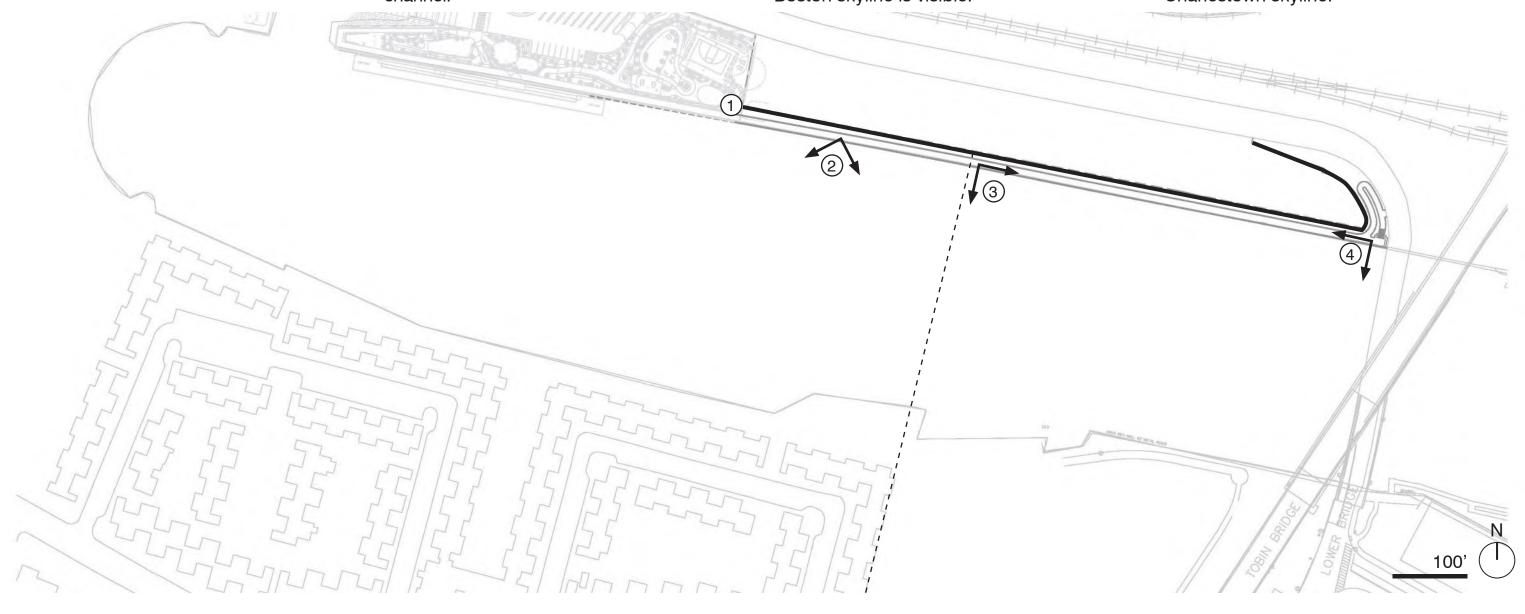
Undesirable view of parking lot and Autoport should be screened along site's north side.



The CharlesNewtown apartments feel quite close when viewed from straight across the channel.



Past the apartments, a wider view of Barry Field, Tobin Bridge, the harbor edge, and the Boston skyline is visible.



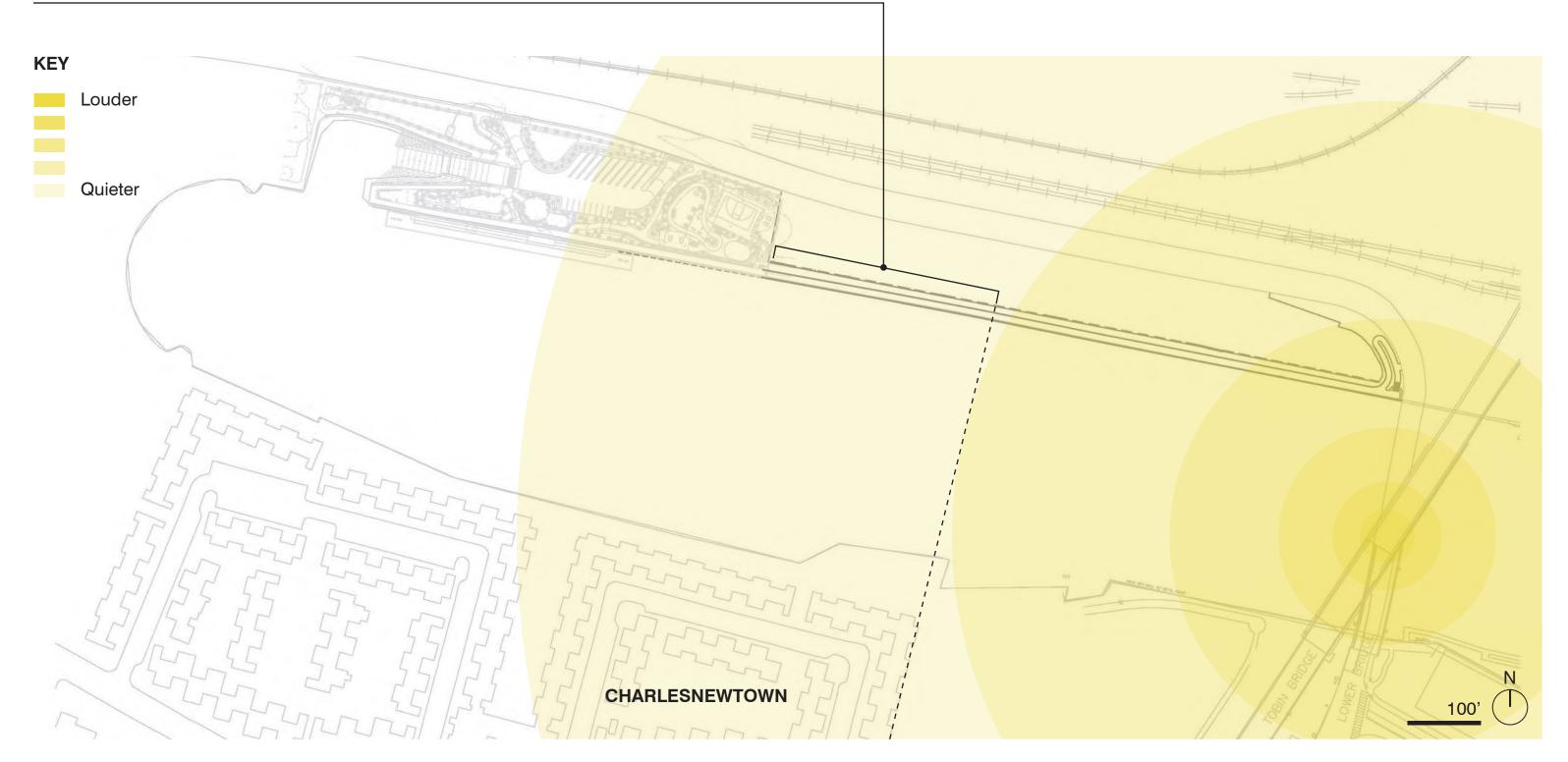


Standing by the bridge gives a long view of the whole Little Mystic Channel & Charlestown skyline.



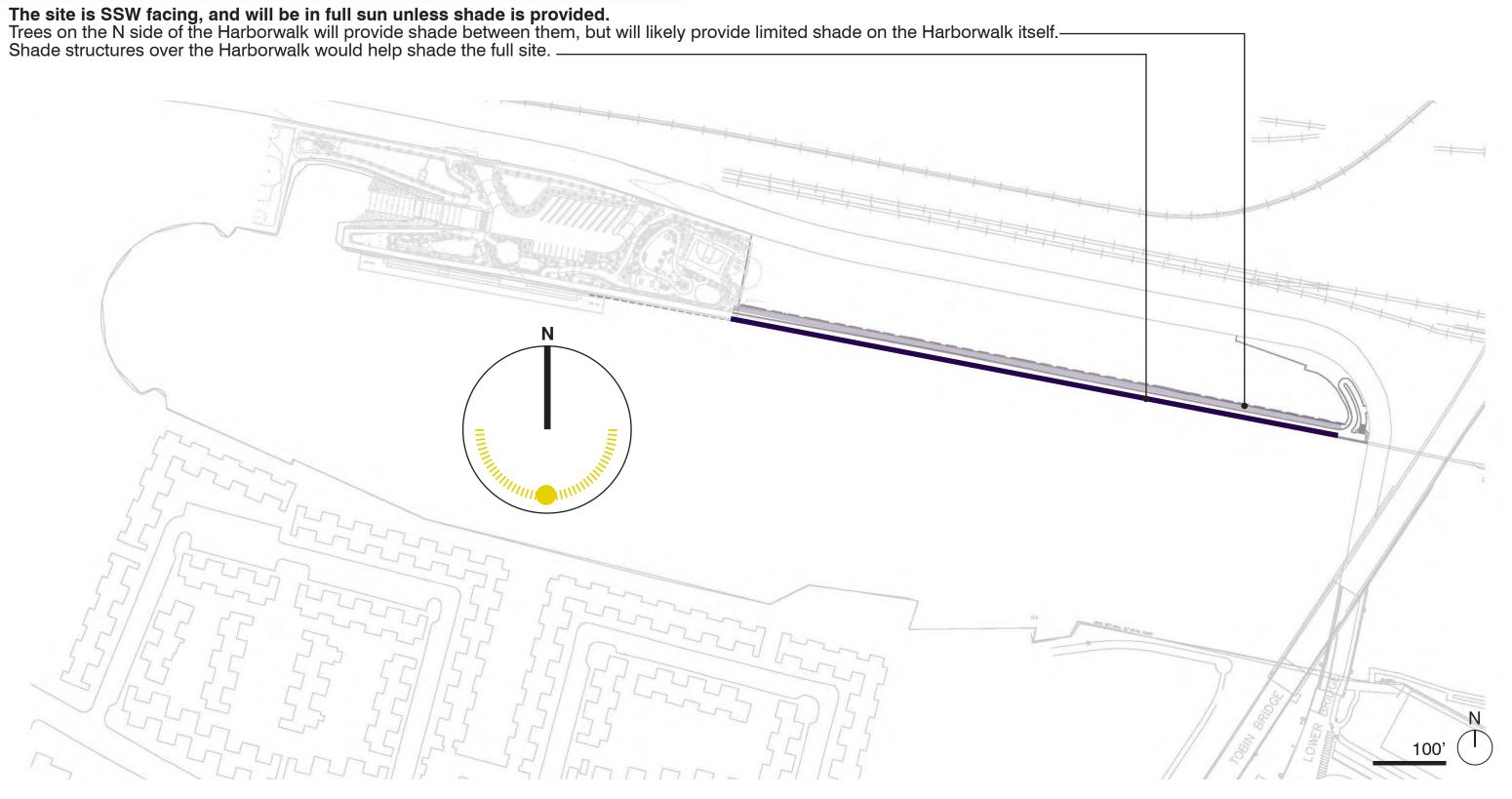
Little Mystic Channel Harborwalk Extension **SITE CONDITIONS - SOUND**

Traffic noise from Tobin Memorial Bridge is quite noticeable on the east side of the site. The portion of the site across from CharlesNewtown feels quieter & more sheltered. This area may be more suitable for gathering spaces or quiet seating areas.





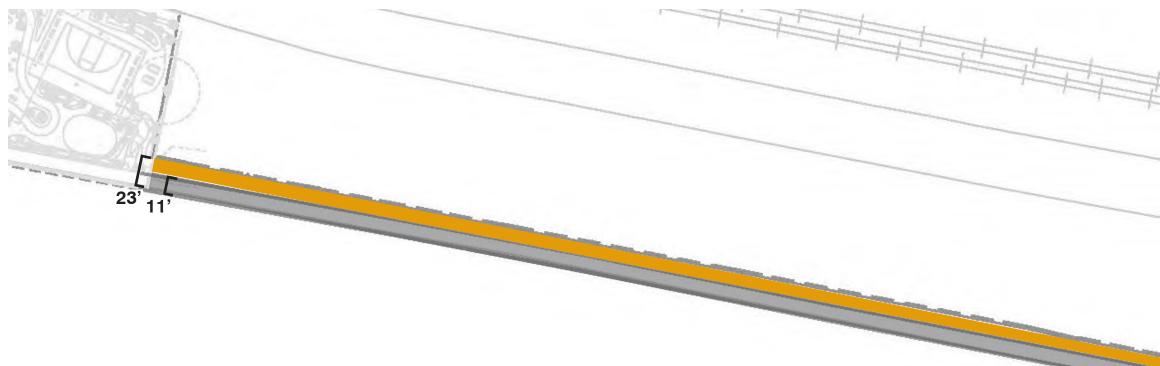
Little Mystic Channel Harborwalk Extension **SITE CONDITIONS -** SUN & SHADE





Little Mystic Channel Harborwalk Extension **SITE CONDITIONS - SPACE**

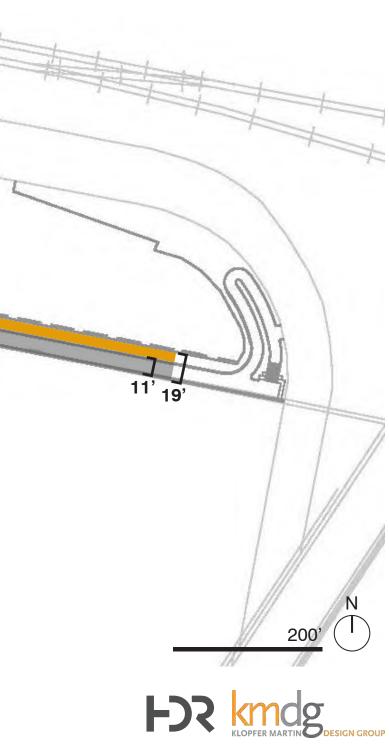
From west to east, the site's width tapers from 23' to 19', before widening near the bridge. Amenities that require slightly more space would need to be placed on the west side of the site, or by the bridge.





Harborwalk Planting & Amenities

January 2024

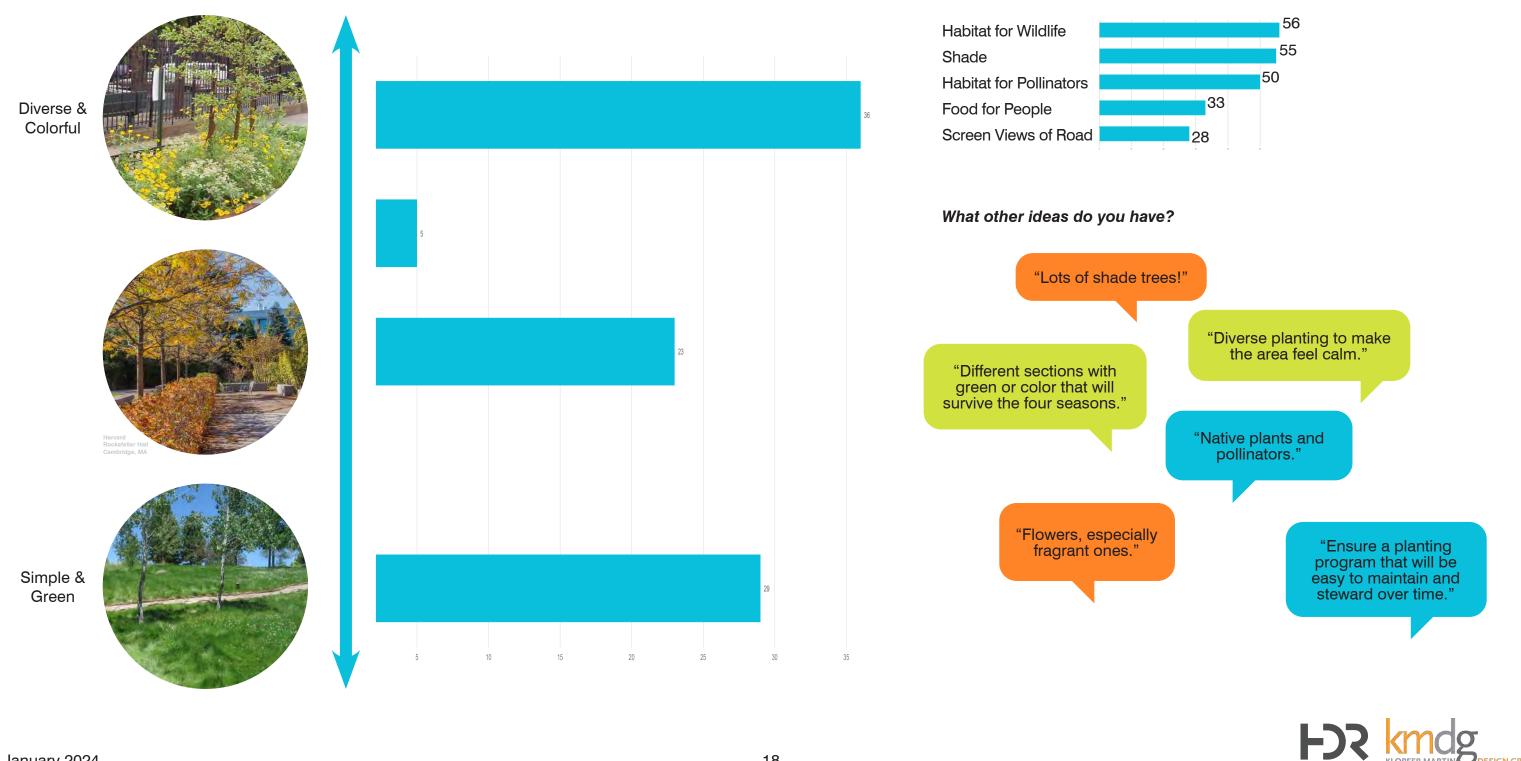


Little Mystic Channel Harborwalk Extension **PLANTING**

COMMUNITY PRIORITIES

Summer '23 Outreach Events & Survey

What style of planting would you most enjoy seeing along the Harborwalk Extension?



What types of planting are most important to you?

Little Mystic Channel Harborwalk Extension MATERIALS & FURNISHINGS - FENCE

CUT & CLIMB-RESISTANT FENCING + DECORATIVE ELEMENTS TO SCREEN PARKING LOT AND INCREASE VISUAL INTEREST





Little Mystic Channel Harborwalk Extension MATERIALS & FURNISHINGS - SHADE STRUCTURES

STEEL OR ALUMINUM STRUCTURES WITH WOOD, STEEL, OR ALUMINUM CANOPIES





Little Mystic Channel Harborwalk Extension MATERIALS & FURNISHINGS - EXERCISE EQUIPMENT

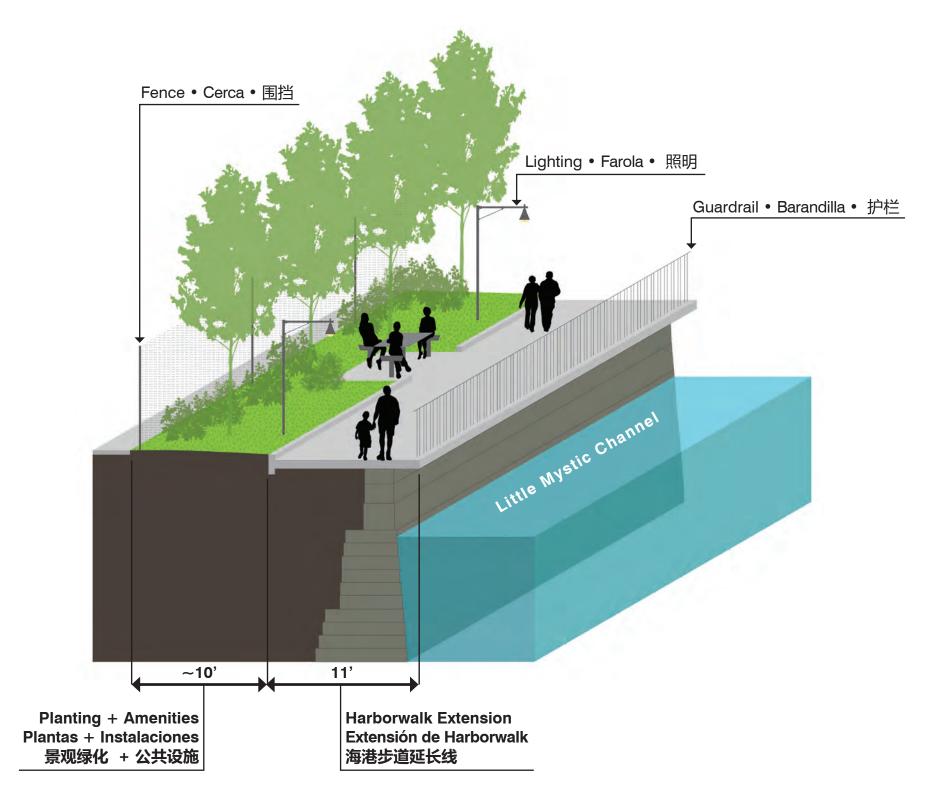
DURABLE EXERCISE EQUIPMENT FOR PEOPLE OF ALL AGES



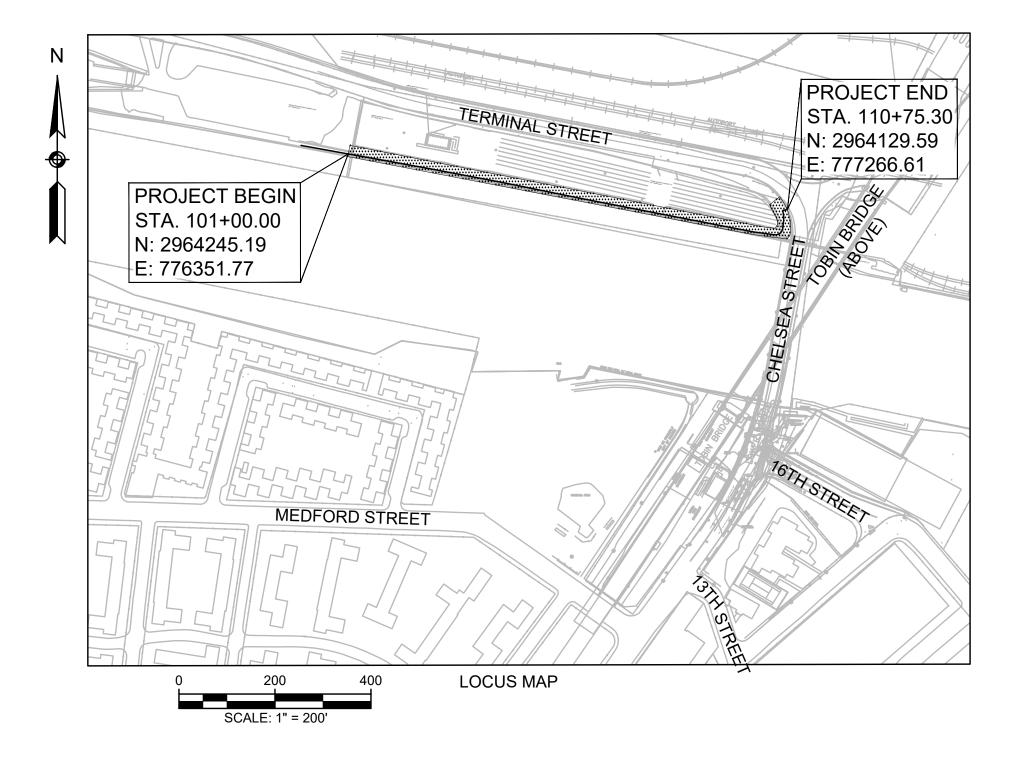


Sit-up Bench





CITY OF BOSTON PUBLIC WORKS DEPARTMENT HONORABLE MICHELLE WU - MAYOR JASCHA FRANKLIN-HODGE - CHIEF OF STREETS **MORAN ANNEX HARBORWALK EXTENSION** (LITTLE MYSTIC CHANNEL)



IN ACCORDANCE WITH

HDR ENGINEERING, INC. 99 HIGH STREET, SUITE 2300 Boston, MA 02111-2626

THE BOSTON PUBLIC WORKS DEPARTMENT, STANDARDS AND SPECIFICATIONS, THE 2024 MASSACHUSETTS DEPARTMENT OF TRANSPORTATION (MASSDOT) SPECIFICATIONS FOR HIGHWAY AND BRIDGES, THE 2023 "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, WHERE CONFLICTS EXIST THE BOSTON PUBLIC WORKS DEPARTMENT STANDARDS AND SPECIFICATIONS SHALL GOVERN.



NOT FOR CONSTRUCTION



SHEET NO.	DESCRIPTION
	CIVIL
1 2 3 4 5-6 7-8 9-10 11-12 13-14 15-16 17-24	TITLE SHEET GENERAL NOTES, LEGEND & ABBREVIATIONS KEY PLAN TYPICAL SECTIONS CONSTRUCTION DETAILS EXISTING CONDITIONS PLANS CONSTRUCTION BASELINE TIE PLANS CONSTRUCTION PLANS & PROFILE GRADING, DRAINAGE & UTILITY PLANS UTILITY DETAILS SECTIONS
	LANDSCAPING
XX XX XX	GRADING PLANTING PLAN MATERIALS PLAN
	LIGHTING AND ELECTRICAL PLANS
XX	LEGEND/PANELBOARD, LIGHTING FIXTURE, CABLE & CONDUIT SCHEDULES
XX	WALKWAY LIGHTING PLAN 1
XX	WALKWAY LIGHTING PLAN 2
XX XX	LIGHTING CONTROL CABINET & ELECTRC SERVICE DETAILS UNDERGROUND CONDUIT & FOUNDATION
XX	DETAILS LIGHTING CONTROL SCHEME
	STRUCTURAL PLANS
XX XX XX XX XX	STRUCTURAL GENERAL NOTES PLAN AND ELEVATION CROSS SECTION DETAILS SHEET

APPROVED

SURVEY NOTES

1. SURVEY WAS COMPLETED BY MASSPORT SURVEY USING THE NORTHERN AMERICAN DATUM OF 1983 (2011 ADJUSTMENT) IN U.S. SURVEY FEET, MASSACHUSETTS MAINLAND ZONE. PROJECT CONTROL POINTS WERE ESTABLISHED USING RTK GPS.

2. ELEVATIONS WERE ORIGINALLY NOTED USING THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) BUT WERE LATER ADJUSTED TO BOSTON CITY BASE (BCB) VERTICAL DATUM PLANE. ELEVATIONS SHOWN ARE IN BCB VERTICAL DATUM PLANE.

3. UPON MOBILIZATION THE CONTRACTOR SHALL CONTACT MASSPORT SURVEY (617) 561-1799 FOR ASSISTANCE IN THE RECOVERY OF PROJECT SURVEY CONTROL POINTS.

GENERAL NOTES

4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY "DIG-SAFE" AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION WITHIN THE PROJECT AREA.

5. ALL SITE FEATURES WHICH ARE TO BE DISPOSED OF, INCLUDING EXISTING PAVEMENT, SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL LOCAL, STATE AND FEDERAL REGULATIONS.

6. ALL AREAS DISTURBED BY CONSTRUCTION SHALL BE RESTORED TO THEIR ORIGINAL CONDITION, UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS. EXISTING AND NEWLY CONSTRUCTED DRAINAGE SYSTEMS IN THE PROXIMITY OF THE CONSTRUCTION SHALL BE LEFT CLEAN AND IN GOOD OPERABLE CONDITION.

7. NO EXCAVATIONS SHALL BE LEFT UNPROTECTED AT THE END OF ANY WORK PERIOD. A STEEL PLATE OR DECKING SHALL BE TEMPORARILY PLACED OVER ALL EXCAVATIONS WHEN NOT ACTIVELY IN USE.

8. ALL EXISTING FEATURES WHICH ARE "TO REMAIN" AND WHICH ARE DISTURBED BY THE CONTRACTOR SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.

9. ALL RIMS, COVERS, GRATES AND OTHER CASTINGS OF EXISTING MUNICIPAL UTILITIES THAT ARE LOCATED IN AREAS OF RECONSTRUCTION SHALL BE ADJUSTED TO MATCH FINAL GRADE. ALL PRIVATE UTILITY CASTINGS SHALL BE ADJUSTED OR REMOVED BY OTHERS.

10. SAW CUTTING WILL BE REQUIRED FOR THE PROPOSED DRAINAGE IN THE AUTOPORT PARKING AREA AND A NEAT LINE IS REQUIRED. ALL SAW CUTTING SHALL BE CONSIDERED INCIDENTAL TO THE WORK PERFORMED ON THIS PROJECT. NO SEPARATE PAY ITEM SHALL BE MADE FOR SAW CUTTING.

EXISTING CONDITIONS

11. DUE TO THE ADJACENT BOSTON TRANSPORTATION DEPARTMENT (BTD) CHELSEA STREET INTERSECTION PROJECT, EXISTING CONDITIONS MAY VARY FROM WHAT IS SHOWN. THE CONTRACTOR SHALL VERIFY, PRIOR TO CONSTRUCTION. THE EXISTING CONDITIONS WITHIN THE PROJECT AREA AND IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES WHICH ARE FOUND.

12. SEE "EXISTING CONDITIONS PLAN" FOR ADDITIONAL INFORMATION.

UTILITIES

13. ALL UTILITY OWNERS SHALL HAVE ACCESS TO THEIR EXISTING MANHOLES AND STRUCTURES AT ALL TIMES.

14. THE CONTRACTOR SHALL INSTALL "DON'T DUMP" PLACARDS IN THE SIDEWALK FOR EACH EXISTING AND NEW CATCH BASIN LOCATED WITHIN THE PROJECT AREA. COORDINATE WITH BOSTON WATER AND SEWER COMMISSION FOR LOCATIONS AND INSTALLATION STANDARDS.



LAYOUT AND ALIGNMENT

16. ALL BASELINE TIES FOR CURB CORNERS AND RADII ARE TO THE P.C.'S OR P.T.'S UNLESS OTHERWISE NOTED. WHERE PROPOSED GRANITE CURBING MEETS EXISTING CURBING OR EXISTING ROADWAY OR HARBORWALK PAVEMENT EDGES, MINOR FIELD ADJUSTMENTS TO THE DESIGNATED STATION OF THE P.C. OR P.T. FOR THE PROPOSED CURBING MAY BE REQUIRED.

TRAFFIC MANAGEMENT

17. HOURS SHALL BE FROM 7:00 AM TO 3:30 PM MONDAY THROUGH FRIDAY. WORK ON SATURDAY AND SUNDAY SHALL BE FROM 7:00 AM TO 7:00 PM.

18. CONTRACTOR SHALL NOT BLOCK OR IMPACT THE EXISTING PEDESTRIAN CROSSING. CONSTRUCTED BY BTD (UNDER SEPARATE CONTRACT) AND PROVIDE ACCESS AT ALL TIMES.

SYMBOLS EXISTING PROPOSED CATCH BASIN \bigcirc CATCH BASIN CURB INLET WATER QUALITY UNIT DE (E)ELECTRIC MANHOLE ABBREVIATIONS HYDRANT GATE VALVE ABANDON ABAN TRAFFIC MAST ARM FOUNDATION ADJ ADJUST BC \bowtie **TRAFFIC SIGNAL CABINET** BEV. BEVELED BFD * STREET LIGHT BIT. UTILITY POLE -0-0 BPWD 0-0-BTD ELECTRIC HANDHOLE BWSC BWW STREET LIGHT CABINET CA/T CB SEWER CEM CEMENT CIRC DRAINAGE CIT CHAINLINK FENCE CLF CNO **GUARDRAIL WITH WOOD POSTS** CONC HANDRAIL CONST DH DI **PROPERTY LINE** _____ P _____ D.I. DMH CURB LINE EL. EMH ELECTRIC CONDUIT ____ _ _ _ _ _ UE _____ EOB EΧ EXISTING WATER EXIST EXISTING FC FEDEX GAS _____ G ____ G ____ G ____ ____G___G____G____G___ GC GG _____ COMM _____ COMM _____ COMM _____ **TELEPHONE/COMMUNICATION** GM GR GRS ELECTRICAL GV HH RAILROAD TRACKS HMA

BOSTON WATER AND SEWER COMMISSION GENERAL CONSTRUCTION REQUIREMENTS

ALL MATERIALS USED FOR MODIFICATIONS TO THE DRAINAGE SYSTEM SHALL BE IN ACCORDANCE WITH BOSTON WATER AND SEWER COMMISSION STANDARDS SPECIFICATIONS AND CONSTRUCTION DETAILS.

19. THE CONTRACTOR SHALL PROVIDE, MAINTAIN AND OPERATE ALL TEMPORARY DRAINAGE FACILITIES SUCH AS BYPASS PIPES AND ALL OTHER LABOR AND EQUIPMENT NECESSARY TO MAINTAIN STORMWATER FLOW TO THE LITTLE MYSTIC CHANNEL DURING CONSTRUCTION.



HS

LL LS

MAX

MB

MIN

MH NO.

OHW

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NP\ NTS BOTTOM OF CURB PB PC **BOSTON FIRE DEPARTMENT** ΡM BITUMINOUS PR(BOSTON PUBLIC WORKS DEPARTMENT PR BOSTON TRANSPORTATION DEPARTMENT PT BOSTON WATER AND SEWER COMMISSION PV(**BOSTON WATER WORKS** CENTRAL ARTERY/TUNNEL PROJECT R&[CATCH BASIN R&1 CIRCULAR RE CHANGE IN TYPE RIM SD\ CHAIN LINK FENCE COULD NOT OPEN SMI ST CONCRETE CONSTRUCTION STA STR DRILL HOLE DROP INLET TC TC **DUCTILE IRON PIPE** DRAIN MANHOLE TMI TR ELEVATION ELECTRIC MANHOLE TYP EDGE OF BITUMINOUS USF VGC WC FACE OF CURB WG FEDERAL EXPRESS **GRANITE CURB** GAS GATE VALVE GUTTER MOUTH GUARD RAIL GUARD RAIL SYSTEM GATE VALVE HANDHOLE HOT MIX ASPHALT HIGH SERVICE LEFT LEVEL LANDING LOW SERVICE MAXIMUM MAILBOX MINIMUM MANHOLE NUMBER ORDINARY HIGH WATER MASSPORT SURVEY UNIT SURVEY BY C. ANDERSON DRAWN BY NOT TO SCALE SCALE: A. BEAULAC CHECKED BY DISTRICT: APPROVED BY R. LAROCHELLE CITY ENGINEER

TREE LEGEND

EXISITING (AS NOTED)

PROPOSED XXXXXX

ABBREVIATIONS (CONT.)

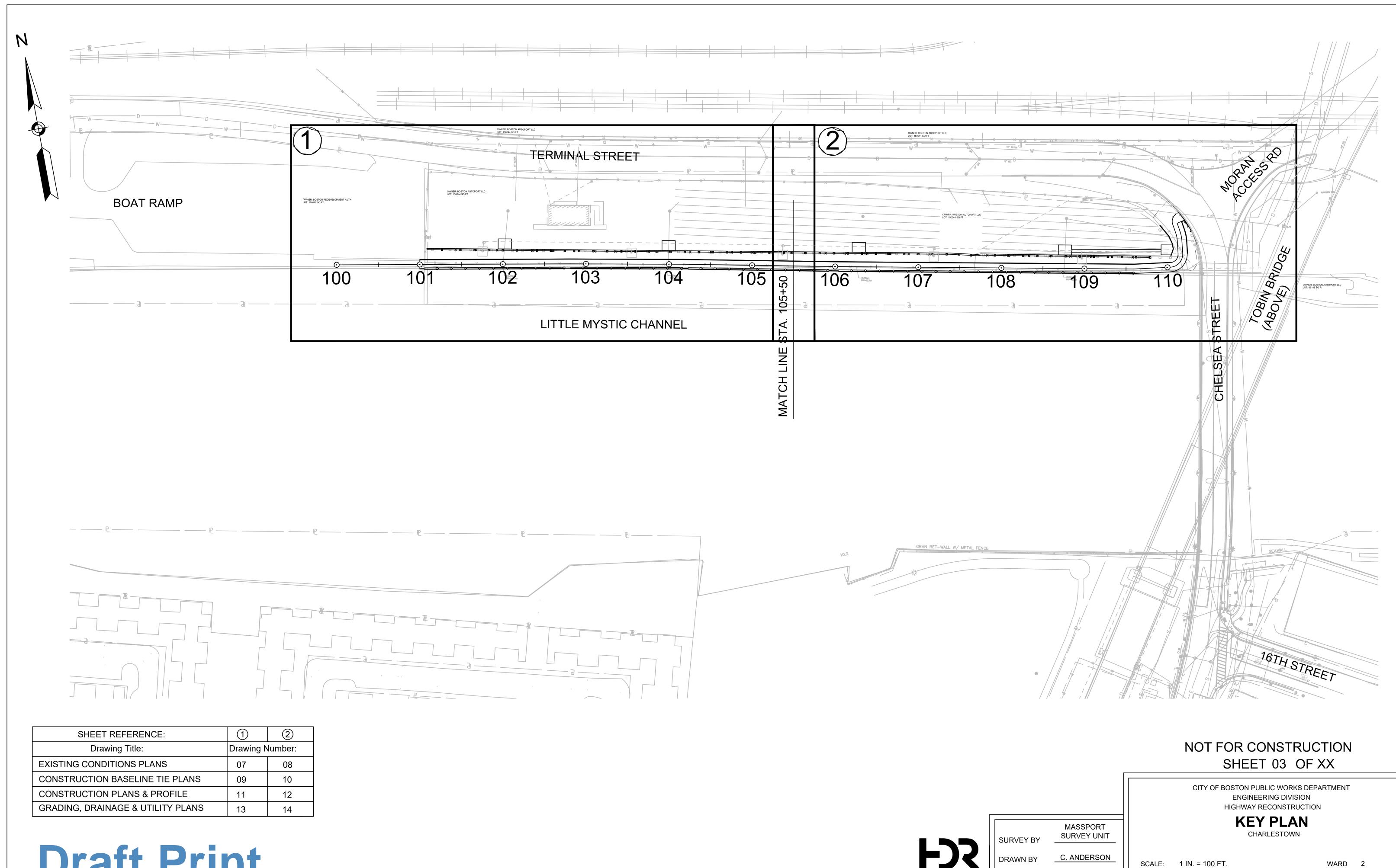
V S	NOT POSSIBLE TO VERIFY NOT TO SCALE
	PULLBOX
	POINT OF CURVATURE
	PARKING METER
С	POINT OF REVERSE CURVATURE
OP	PROPOSED
	POINT OF TANGENCY
С	POLYVINYL CHLORIDE
	RIGHT
D	REMOVE AND DISPOSE
Т	REMOVE AND TRANSPORT
P	REINFORCED CONCRETE PIPE
Т	RETAIN
Λ	RIM ELEVATION
WK	SIDEWALK
IH	SEWER MANHOLE
LT	STREET LIGHT
A	STATION
R	STRAIGHT
	TOP OF CURB
	TRAFFIC CONTROL
IH	TELEPHONE MANHOLE
	TRAFFIC
P	TYPICAL
PS	UNITED STATES POSTAL SERVICE
iC	VERTICAL GRANITE CURB
CR	WHEELCHAIR RAMP
3	WATER GATE VALVE

NOT FOR CONSTRUCTION SHEET 02 OF XX

CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

GENERAL NOTES, LEGEND & ABBREVIATIONS

WARD 2 DATE: DEC 2024

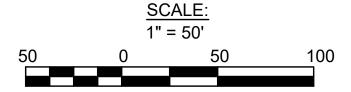


SHEET REFERENCE:	1	2
Drawing Title:	Drawing N	lumber:
EXISTING CONDITIONS PLANS	07	08
CONSTRUCTION BASELINE TIE PLANS	09	10
CONSTRUCTION PLANS & PROFILE	11	12
GRADING, DRAINAGE & UTILITY PLANS	13	14

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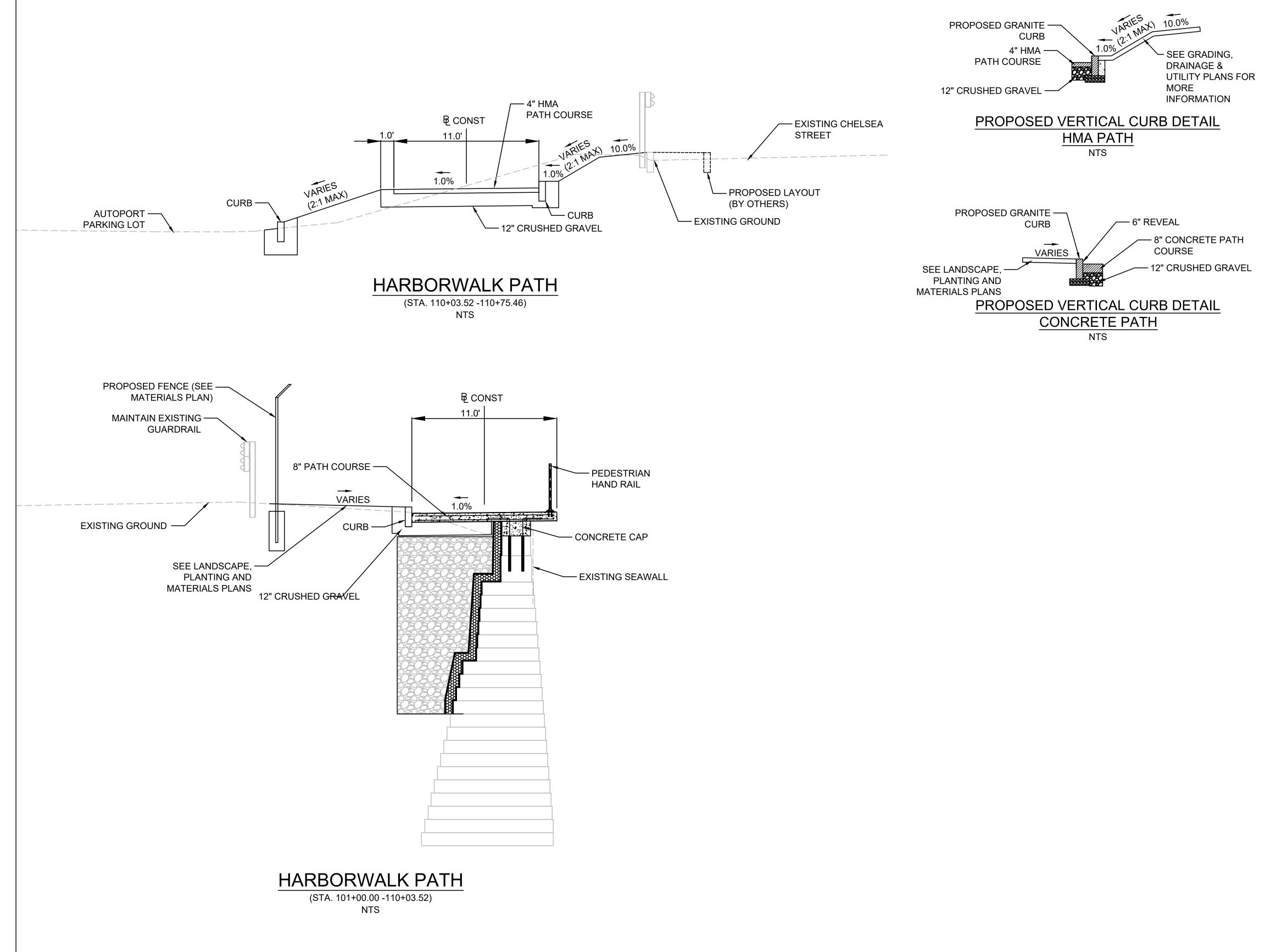
CHECKED BY



A. BEAULAC DISTRICT: 1 APPROVED BY R. LAROCHELLE

CITY ENGINEER

WARD 2 DATE: DEC 2024



GENERAL NOTES

- 1. SEE CONSTRUCTION PLANS FOR LOCATION OF CURB
- 2. SEE LANDSCAPE, PLANTING AND MATERIALS PLANS FOR HARBORWALK MATERIALS AND OTHER REQUIREMENTS.
- 3. SEE GRADING PLANS FOR TIE IN TO CHELSEA STREET.





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DRAWN BY CHECKED BY <u>A. BEAULAC</u> APPROVED BY R. LAROCHELLE

CITY ENGINEER

NOT TO SCALE

DISTRICT: 1

SCALE:

NOT FOR CONSTRUCTION

CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION

HIGHWAY RECONSTRUCTION

TYPICAL SECTIONS

CHARLESTOWN

SHEET 04 OF XX

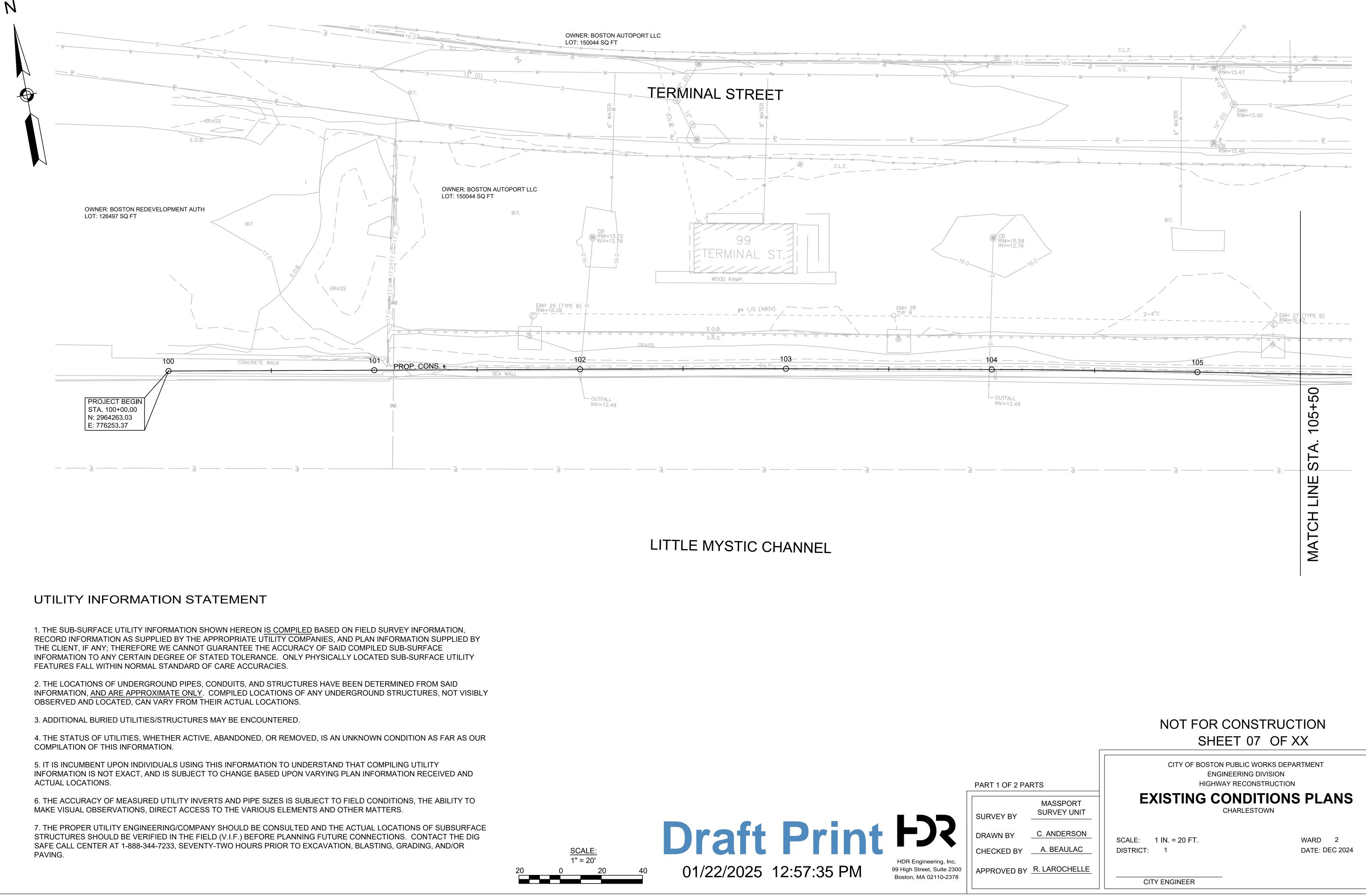
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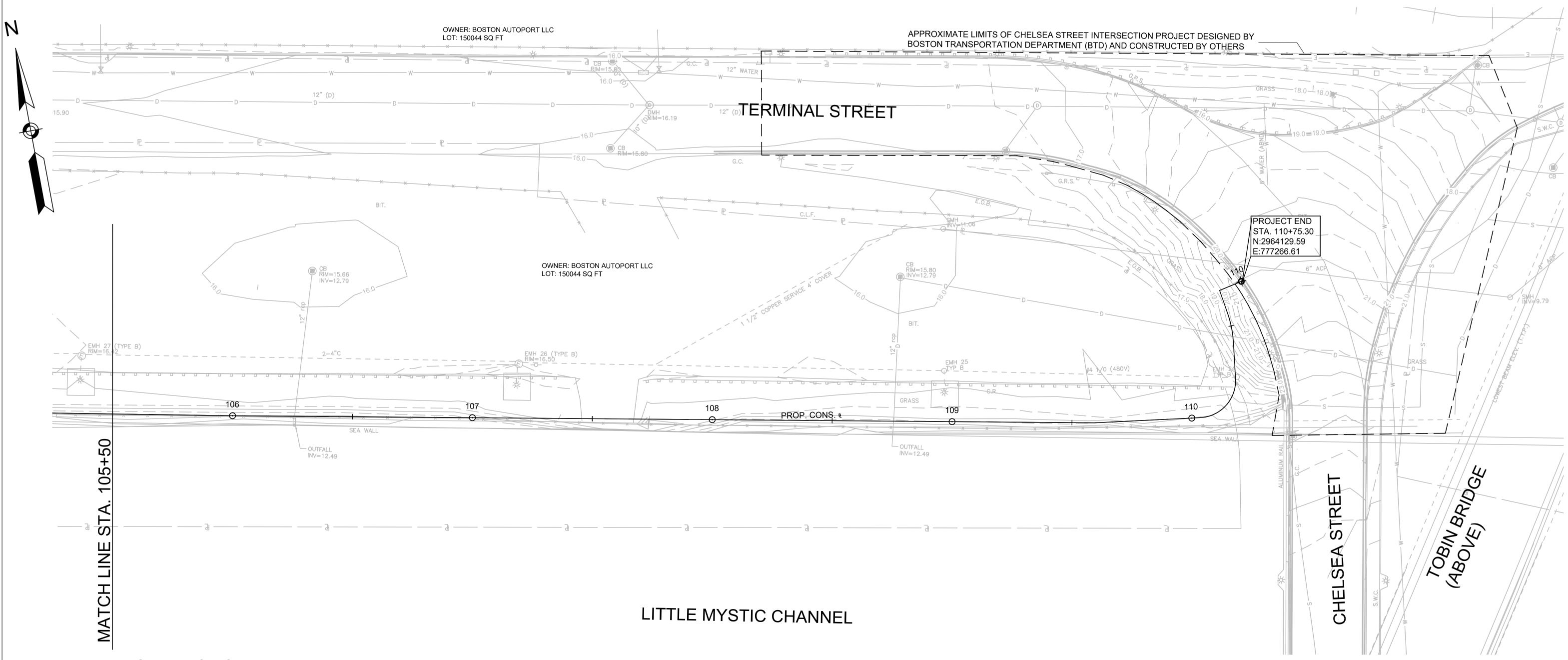
MASSPORT

SURVEY BY

C. ANDERSON

SURVEY UNIT





UTILITY INFORMATION STATEMENT

1. THE SUB-SURFACE UTILITY INFORMATION SHOWN HEREON IS COMPILED BASED ON FIELD SURVEY INFORMATION, RECORD INFORMATION AS SUPPLIED BY THE APPROPRIATE UTILITY COMPANIES, AND PLAN INFORMATION SUPPLIED BY THE CLIENT, IF ANY; THEREFORE WE CANNOT GUARANTEE THE ACCURACY OF SAID COMPILED SUB-SURFACE INFORMATION TO ANY CERTAIN DEGREE OF STATED TOLERANCE. ONLY PHYSICALLY LOCATED SUB-SURFACE UTILITY FEATURES FALL WITHIN NORMAL STANDARD OF CARE ACCURACIES.

2. THE LOCATIONS OF UNDERGROUND PIPES, CONDUITS, AND STRUCTURES HAVE BEEN DETERMINED FROM SAID INFORMATION, AND ARE APPROXIMATE ONLY. COMPILED LOCATIONS OF ANY UNDERGROUND STRUCTURES, NOT VISIBLY OBSERVED AND LOCATED, CAN VARY FROM THEIR ACTUAL LOCATIONS.

3. ADDITIONAL BURIED UTILITIES/STRUCTURES MAY BE ENCOUNTERED.

4. THE STATUS OF UTILITIES, WHETHER ACTIVE, ABANDONED, OR REMOVED, IS AN UNKNOWN CONDITION AS FAR AS OUR COMPILATION OF THIS INFORMATION.

5. IT IS INCUMBENT UPON INDIVIDUALS USING THIS INFORMATION TO UNDERSTAND THAT COMPILING UTILITY INFORMATION IS NOT EXACT, AND IS SUBJECT TO CHANGE BASED UPON VARYING PLAN INFORMATION RECEIVED AND ACTUAL LOCATIONS.

6. THE ACCURACY OF MEASURED UTILITY INVERTS AND PIPE SIZES IS SUBJECT TO FIELD CONDITIONS, THE ABILITY TO MAKE VISUAL OBSERVATIONS, DIRECT ACCESS TO THE VARIOUS ELEMENTS AND OTHER MATTERS.

7. THE PROPER UTILITY ENGINEERING/COMPANY SHOULD BE CONSULTED AND THE ACTUAL LOCATIONS OF SUBSURFACE STRUCTURES SHOULD BE VERIFIED IN THE FIELD (V.I.F.) BEFORE PLANNING FUTURE CONNECTIONS. CONTACT THE DIG SAFE CALL CENTER AT 1-888-344-7233, SEVENTY-TWO HOURS PRIOR TO EXCAVATION, BLASTING, GRADING, AND/OR PAVING.





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MASSPORT SURVEY UNIT SURVEY BY C. ANDERSON DRAWN BY SCALE: 1 IN. = 20 FT. A. BEAULAC DISTRICT: CHECKED BY APPROVED BY R. LAROCHELLE

PART 2 OF 2 PARTS

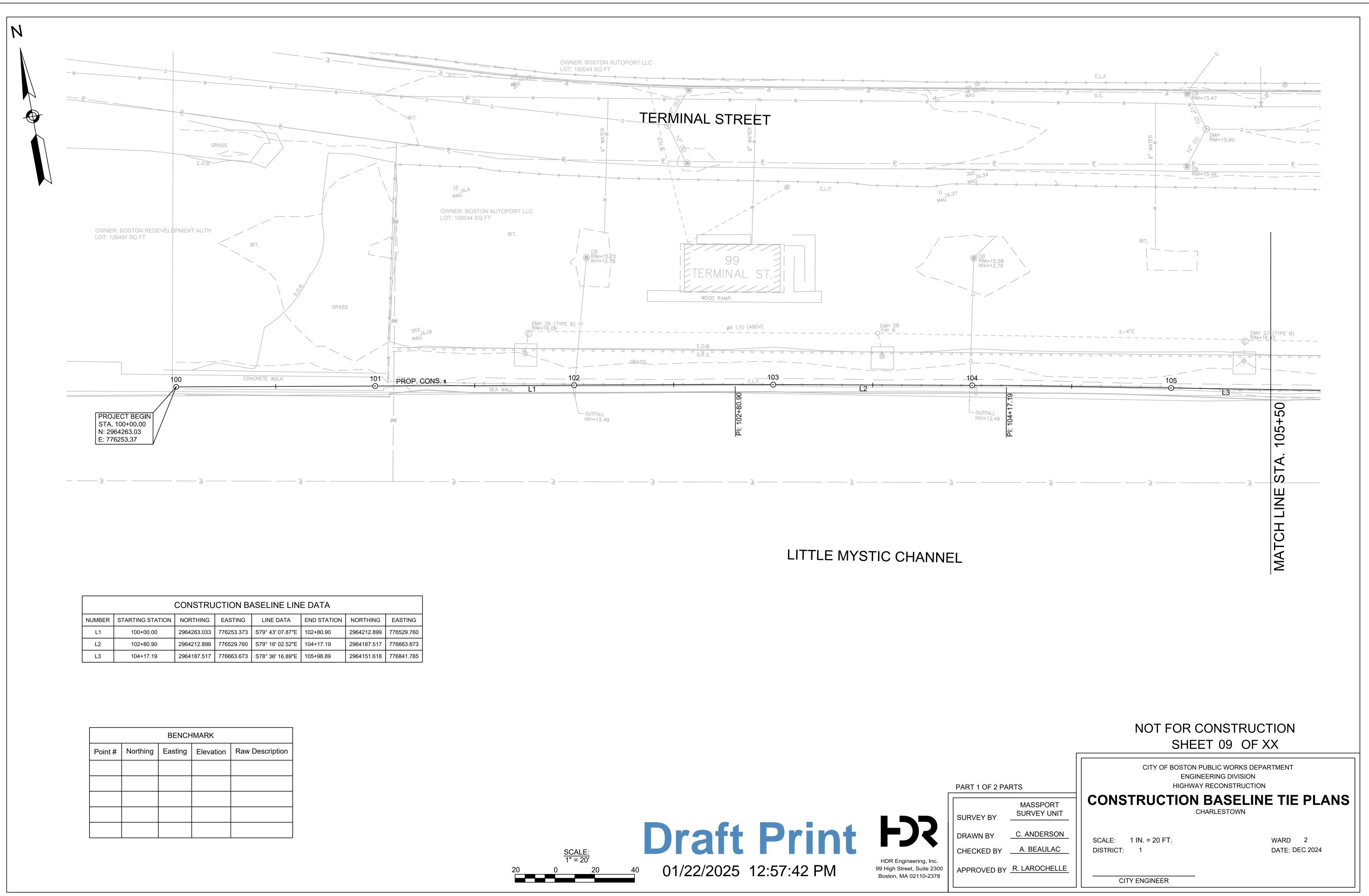
NOT FOR CONSTRUCTION SHEET 08 OF XX

CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

EXISTING CONDITIONS PLANS CHARLESTOWN

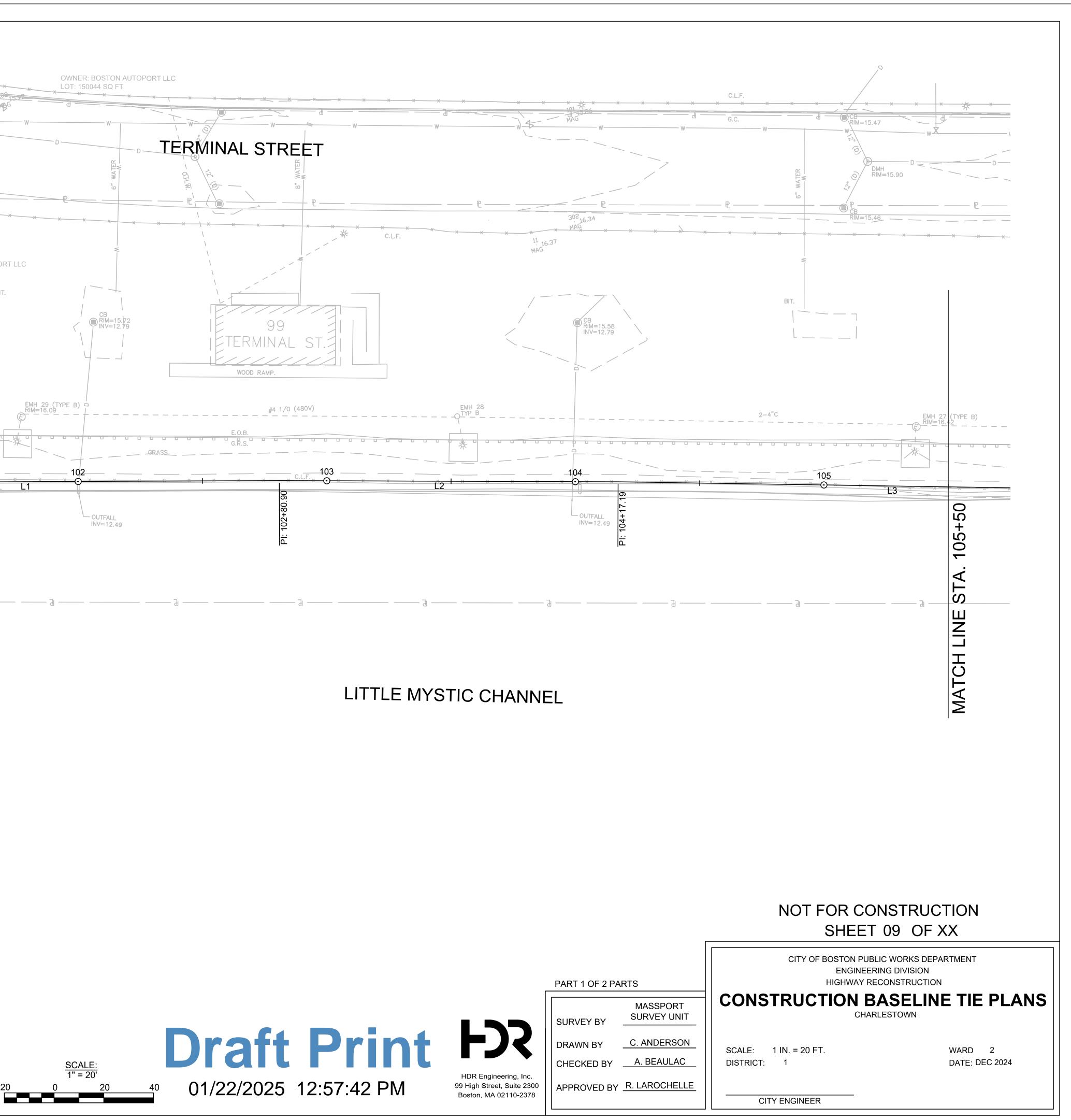
CITY ENGINEER

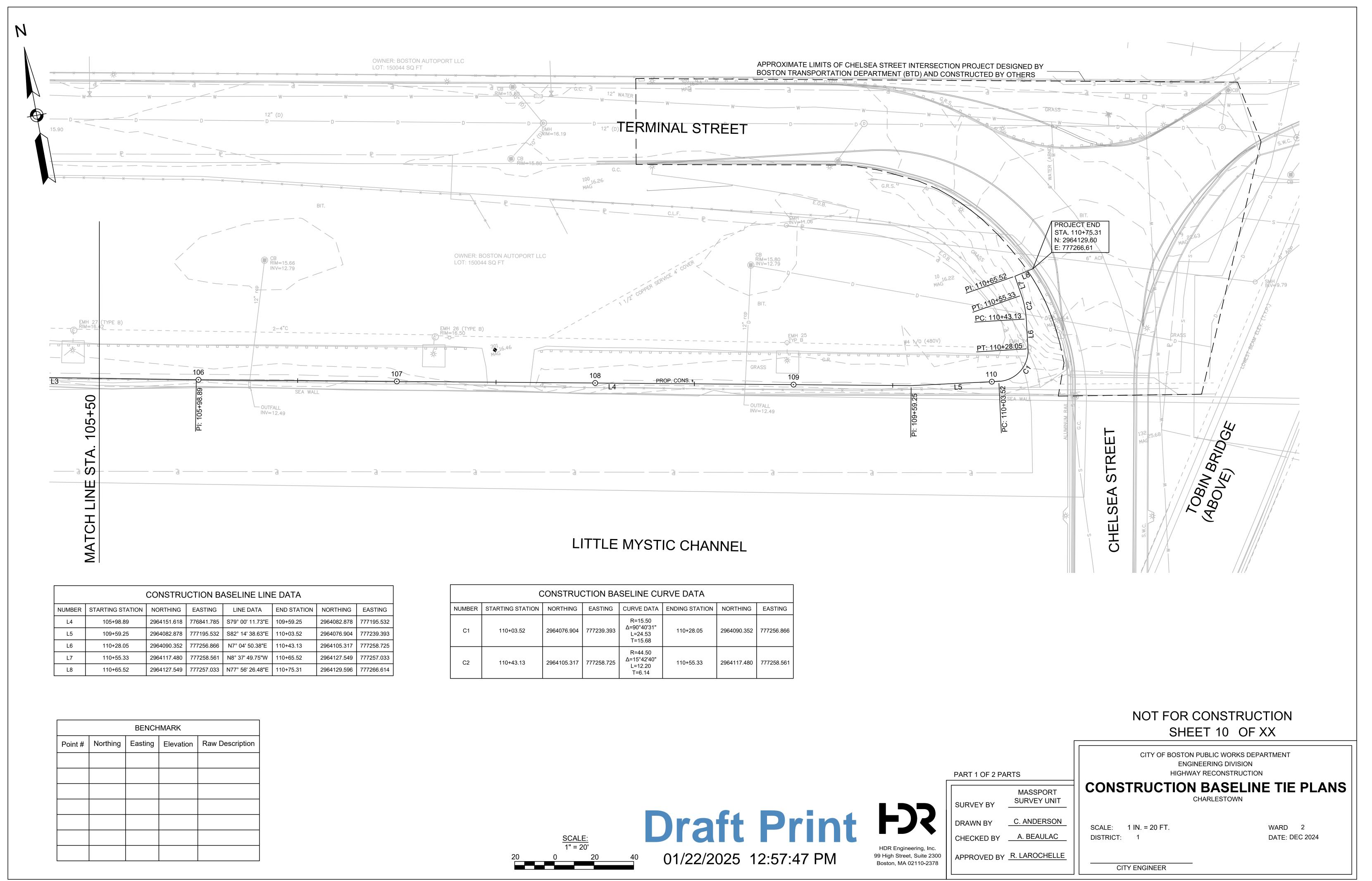
WARD 2 DATE: DEC 2024



	CONSTRUCTION BASELINE LINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	LINE DATA	END STATION	NORTHING	EASTING		
L1	100+00.00	2964263.033	776253.373	S79° 43' 07.87"E	102+80.90	2964212.899	776529.760		
L2	102+80.90	2964212.899	776529.760	S79° 16' 02.52"E	104+17.19	2964187.517	776663.673		
L3	104+17.19	2964187.517	776663.673	S78° 36' 16.89"E	105+98.89	2964151.618	776841.785		

	BENCHMARK								
Point #	Northing	Easting	Elevation	Raw Description					

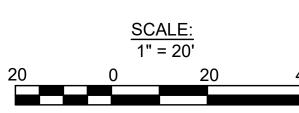




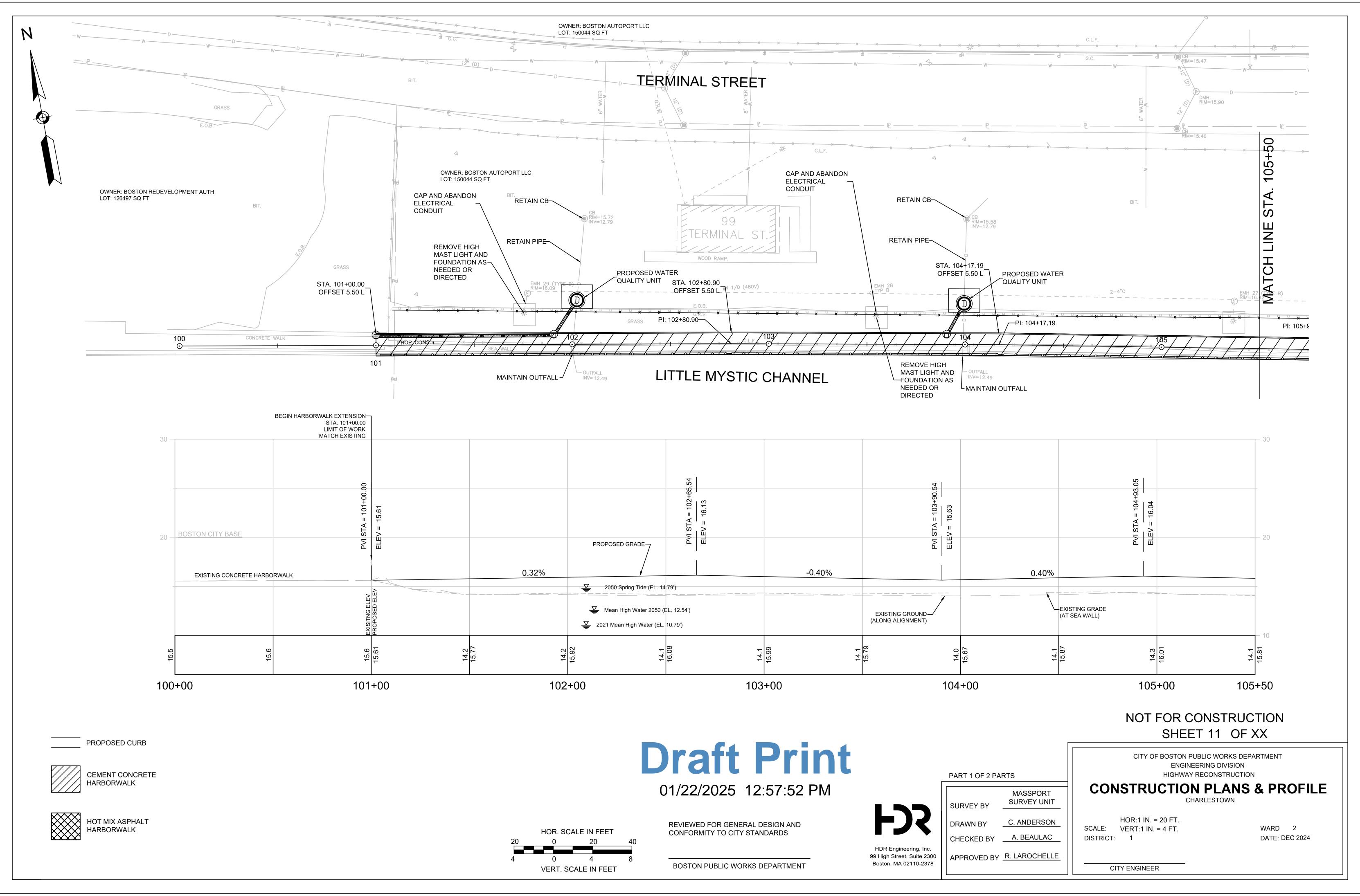
CONSTRUCTION BASELINE LINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	LINE DATA	END STATION	NORTHING	EASTING	
L4	105+98.89	2964151.618	776841.785	S79° 00' 11.73"E	109+59.25	2964082.878	777195.532	
L5	109+59.25	2964082.878	777195.532	S82° 14' 38.63"E	110+03.52	2964076.904	777239.393	
L6	110+28.05	2964090.352	777256.866	N7° 04' 50.38"E	110+43.13	2964105.317	777258.725	
L7	110+55.33	2964117.480	777258.561	N8° 37' 49.75"W	110+65.52	2964127.549	777257.033	
L8	110+65.52	2964127.549	777257.033	N77° 56' 26.48"E	110+75.31	2964129.596	777266.614	

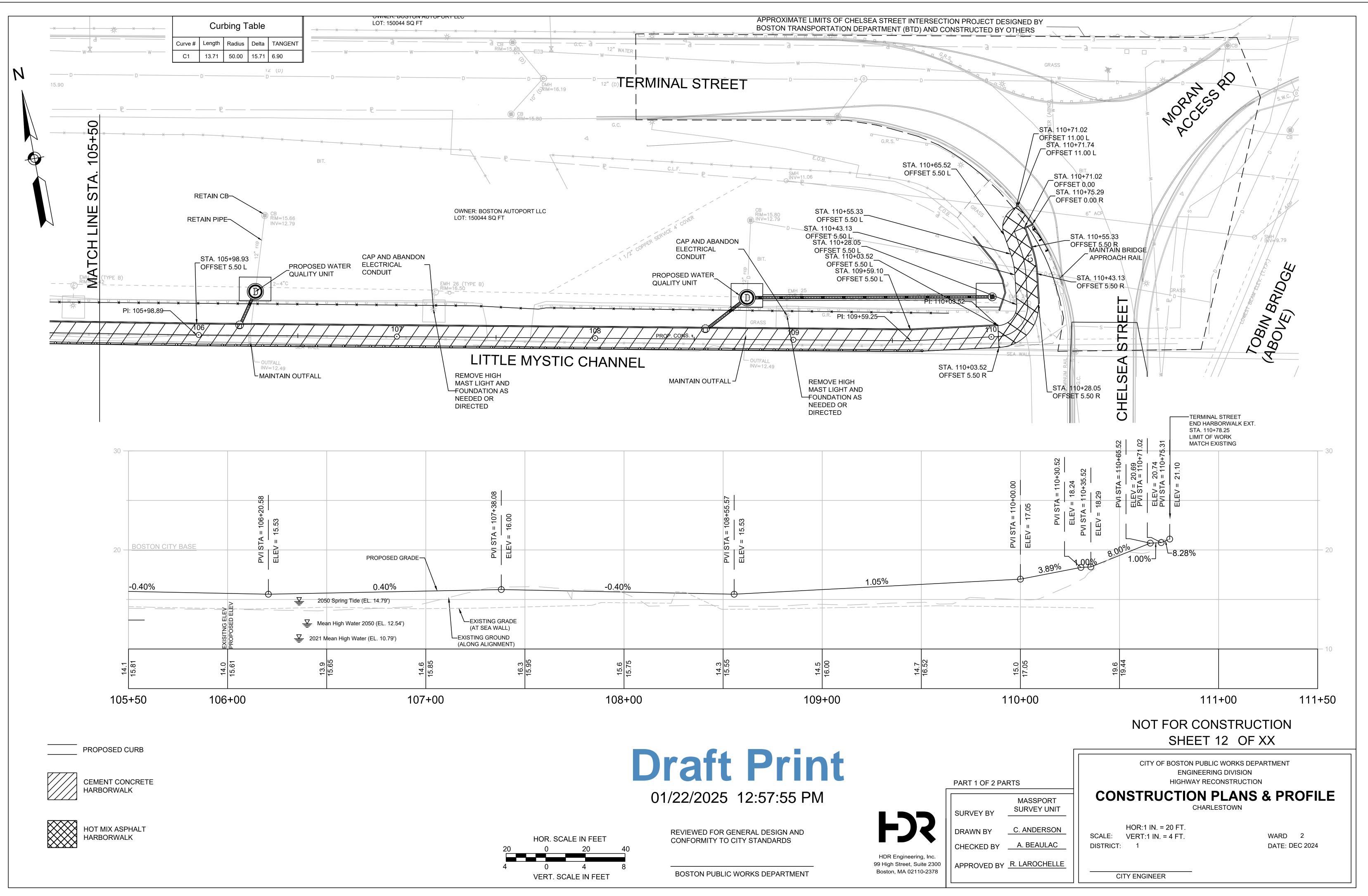
BENCHMARK								
Northing	Easting	Elevation	Raw Description					
	Northing							

CONSTRUCTION BASELINE CURVE DATA									
JMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	ENDING STATION	NORTHING	EASTING		
C1	110+03.52	2964076.904	777239.393	R=15.50 ∆=90°40'31" L=24.53 T=15.68	110+28.05	2964090.352	777256.866		
C2	110+43.13	2964105.317	777258.725	R=44.50 Δ=15°42'40" L=12.20 T=6.14	110+55.33	2964117.480	777258.561		



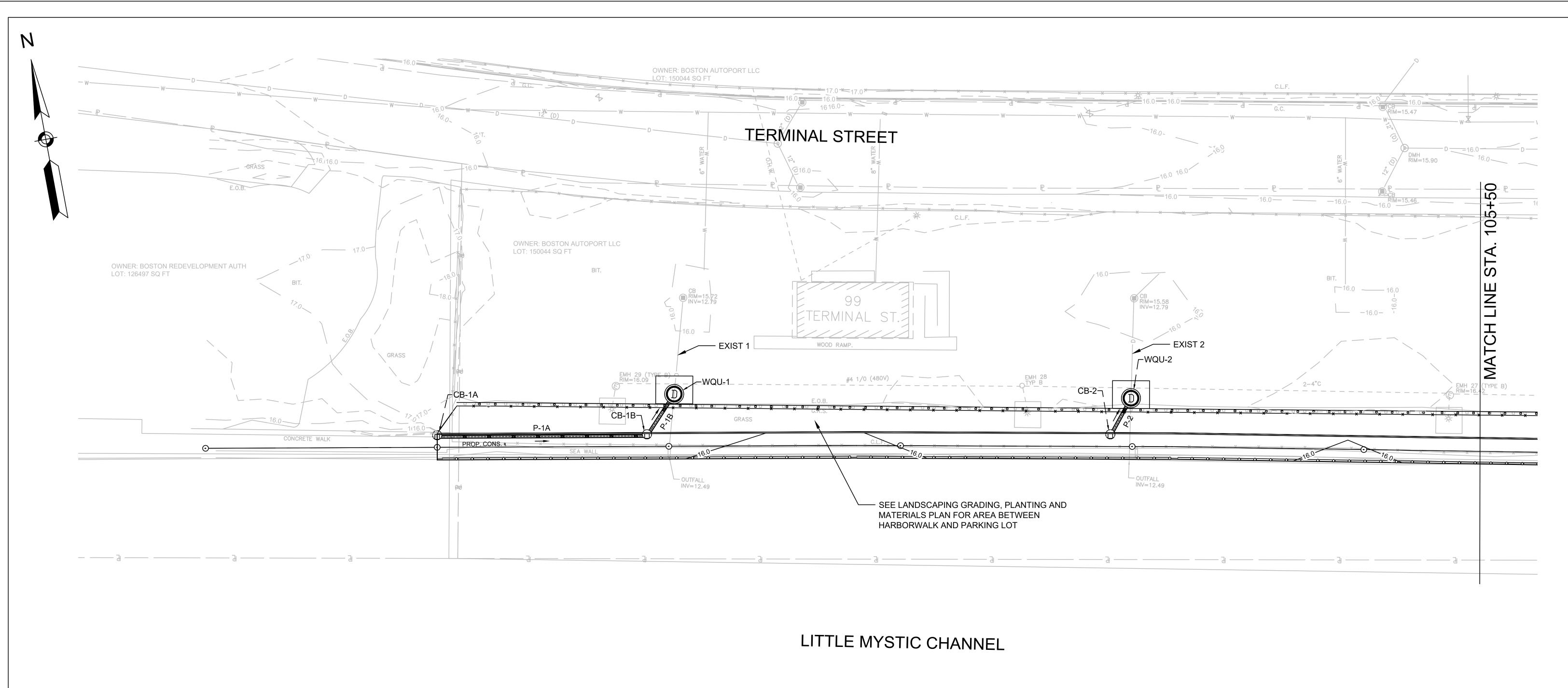












	PIPE SCHEDULE								
NAME	START STATION	END STATION	LENGTH	SIZE	MATERIAL	SLOPE	START INV.	END INV.	
P-1A	101+00.00	101+90.55	90.55	12"	Reinforced Concrete Pipe	0.51%	13.28	12.82	
P-1B	101+90.55	102+02.39	21.58	12"	Reinforced Concrete Pipe	0.46%	12.72	12.62	
P-2	103+90.53	103+99.40	18.59	12"	Reinforced Concrete Pipe	0.54%	12.71	12.61	

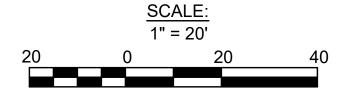
	DRAINAGE STRUCTURE DATA								
NO.	TYPE	STATION	RIM ELEV. (NOTE)	INV. IN	INV. OUT				
CB-1A	CB CURB INLET	101+00.00, (-4.5 L)	15.56		13.28 (P-1A)				
CB-1B	CB CURB INLET	101+90.55, (-4.5 L)	15.85	12.82 (P-1A)	12.72 (P-1B)				
CB-2	CB CURB INLET	103+90.54, (-4.5 L)	15.59		12.71 (P-2)				
WQU-1	WQU	102+02.39, (-22.5 L)	16.41	12.62 (P-1B)* 12.62 (Exist 1)*	12.62 (Exist 1)*				
WQU-2	WQU	103+99.40, (-20.8 L)	16.41	12.61 (P-2) * 12.61 (Exist 2)*	12.61 (Exist 2)*				
*CONTRA	CTOR SHALL FIELD	VERIFY AND MATCH	EXISTING PI	PE ELEVATION.					

GENERAL NOTES

- 1. CONTRACTOR TO ADJUST ALL CITY OWNED UTILITIES TO PROPOSED GRADE.
- 2. CONTRACTOR TO COORDINATE WITH ALL OTHER PRIVATE UTILITIES FOR COVER ADJUSTMENT.



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BOSTON PUBLIC WORKS DEPARTMENT

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NOT FOR CONSTRUCTION SHEET 13 OF XX

CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

GRADING, DRAINAGE & UTILITY PLANS CHARLESTOWN

SCALE: 1 IN. = 20 FT. DISTRICT: 1

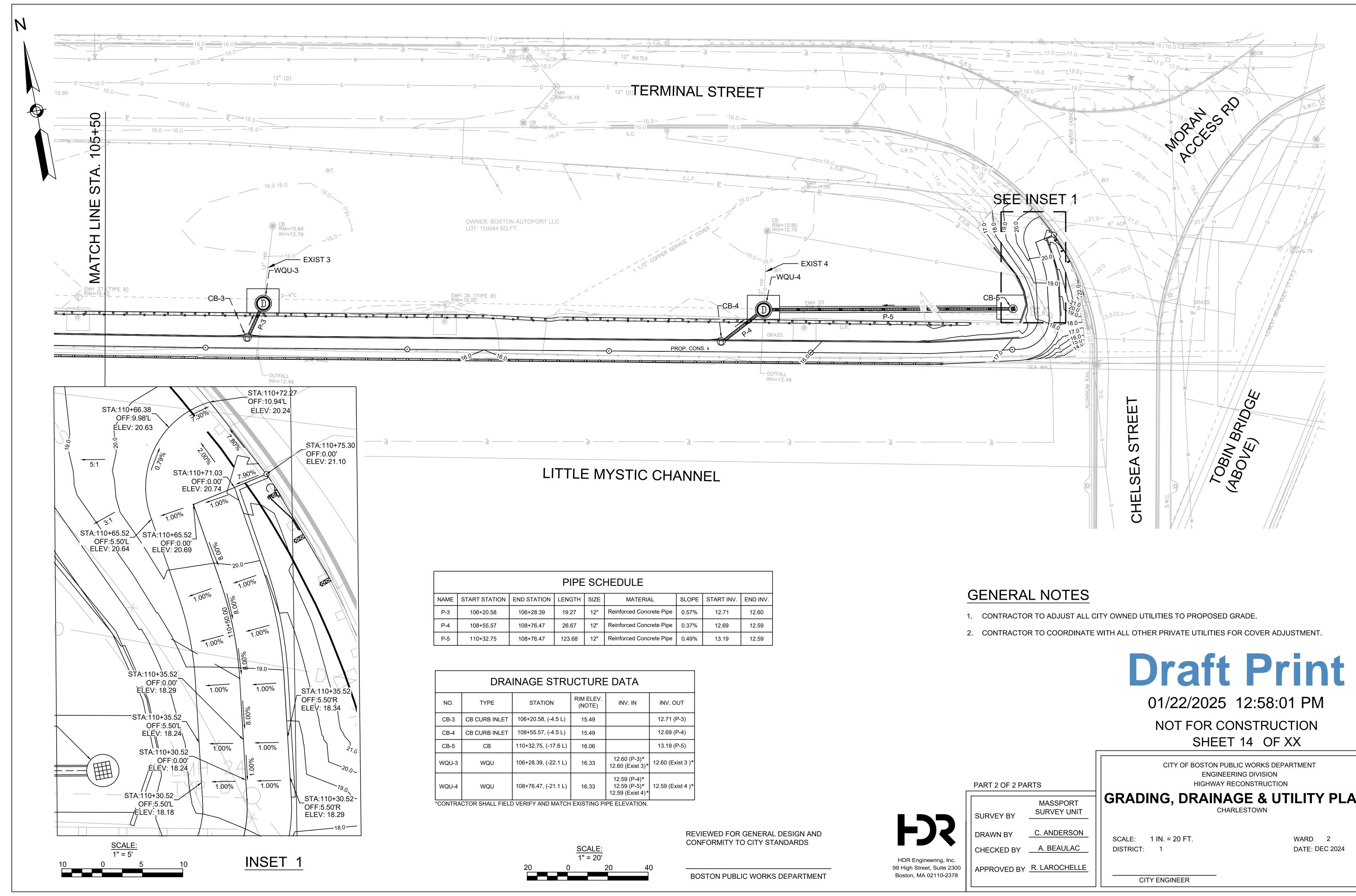
CITY ENGINEER

WARD 2 DATE: DEC 2024

PART 1 OF 2 PARTS

MASSPORT SURVEY UNIT SURVEY BY C. ANDERSON DRAWN BY A. BEAULAC CHECKED BY

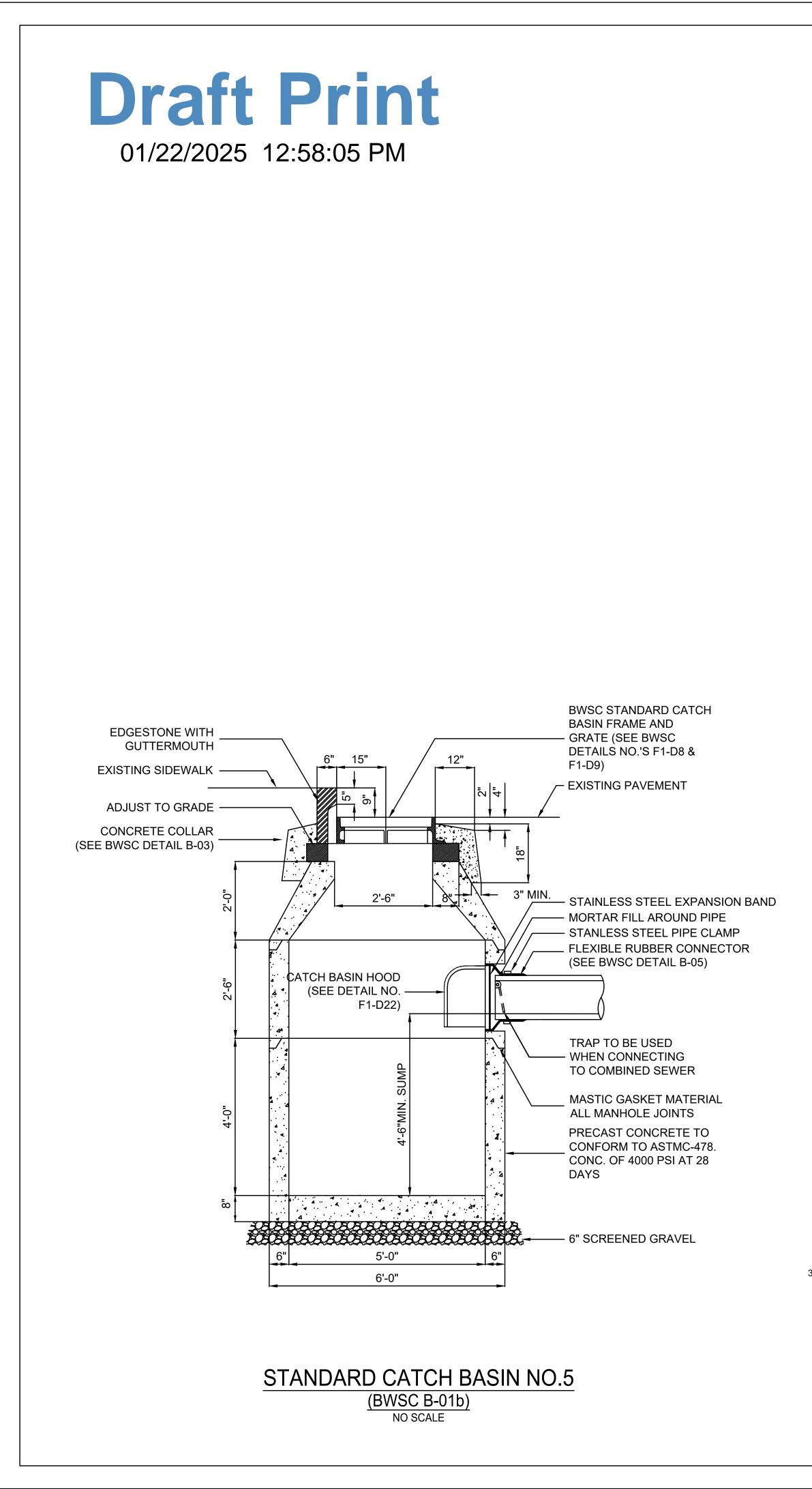
APPROVED BY R. LAROCHELLE

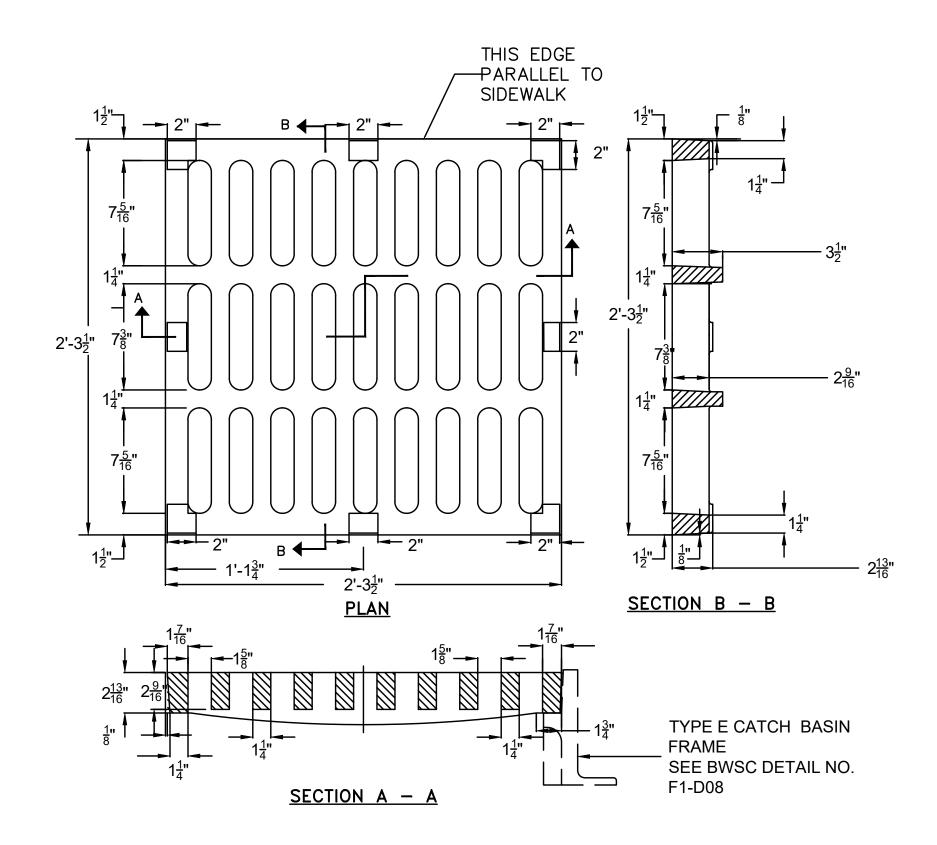


PIPE SCHEDULE									
RT STATION	END STATION	LENGTH	SIZE	MATERIAL	SLOPE	START INV.	END INV.		
06+20.58	106+28.39	19.27	12"	Reinforced Concrete Pipe	0.57%	12.71	12.60		
08+55.57	108+76.47	26.67	12"	Reinforced Concrete Pipe	0.37%	12.69	12.59		
10+32.75	108+76.47	123.68	12"	Reinforced Concrete Pipe	0.49%	13.19	12.59		

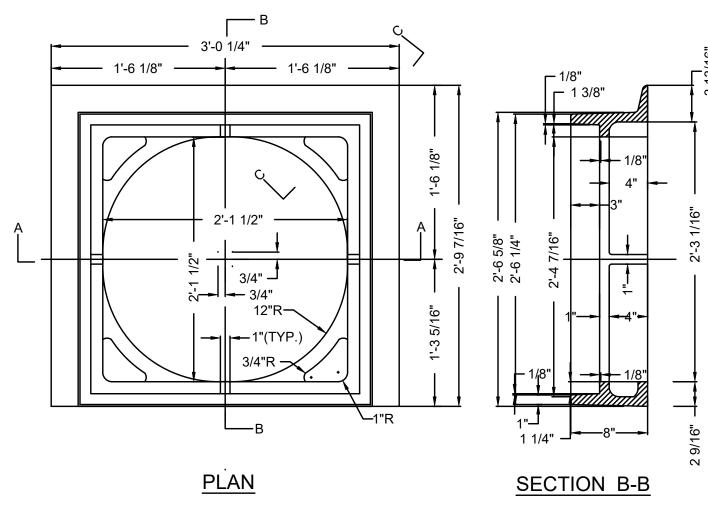
DRAINAGE STRUCTURE DATA								
TYPE	STATION	RIM ELEV. (NOTE)	INV. IN	INV. OUT				
3 CURB INLET	106+20.58, (-4.5 L)	15.49		12.71 (P-3)				
3 CURB INLET	108+55.57, (-4.5 L)	15.49		12.69 (P-4)				
СВ	110+32.75, (-17.6 L)	16.06		13.19 (P-5)				
WQU	106+28.39, (-22.1 L)	16.33	12.60 (P-3)* 12.60 (Exist 3)*	12.60 (Exist 3)*				
WQU	108+76.47, (-21.1 L)	16.33	12.59 (P-4)* 12.59 (P-5)* 12.59 (Exist 4)*	12.59 (Exist 4)*				

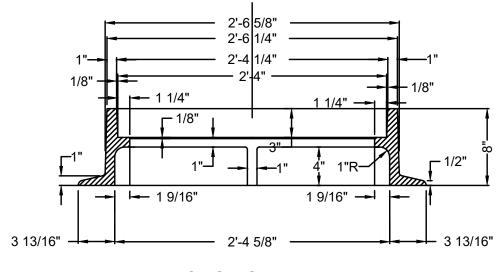
GRADING, DRAINAGE & UTILITY PLANS











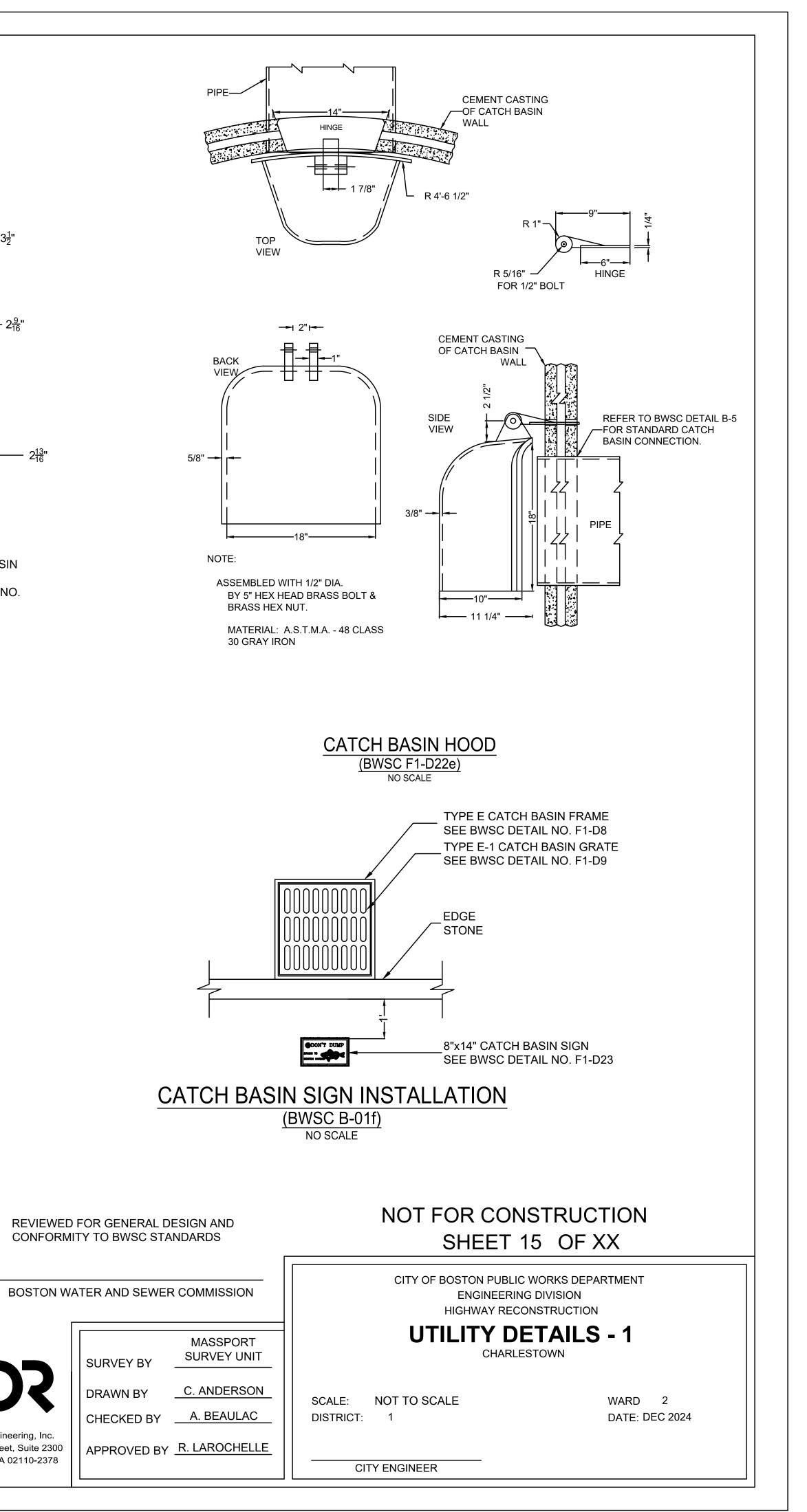
SECTION A-A

SECTION C-C



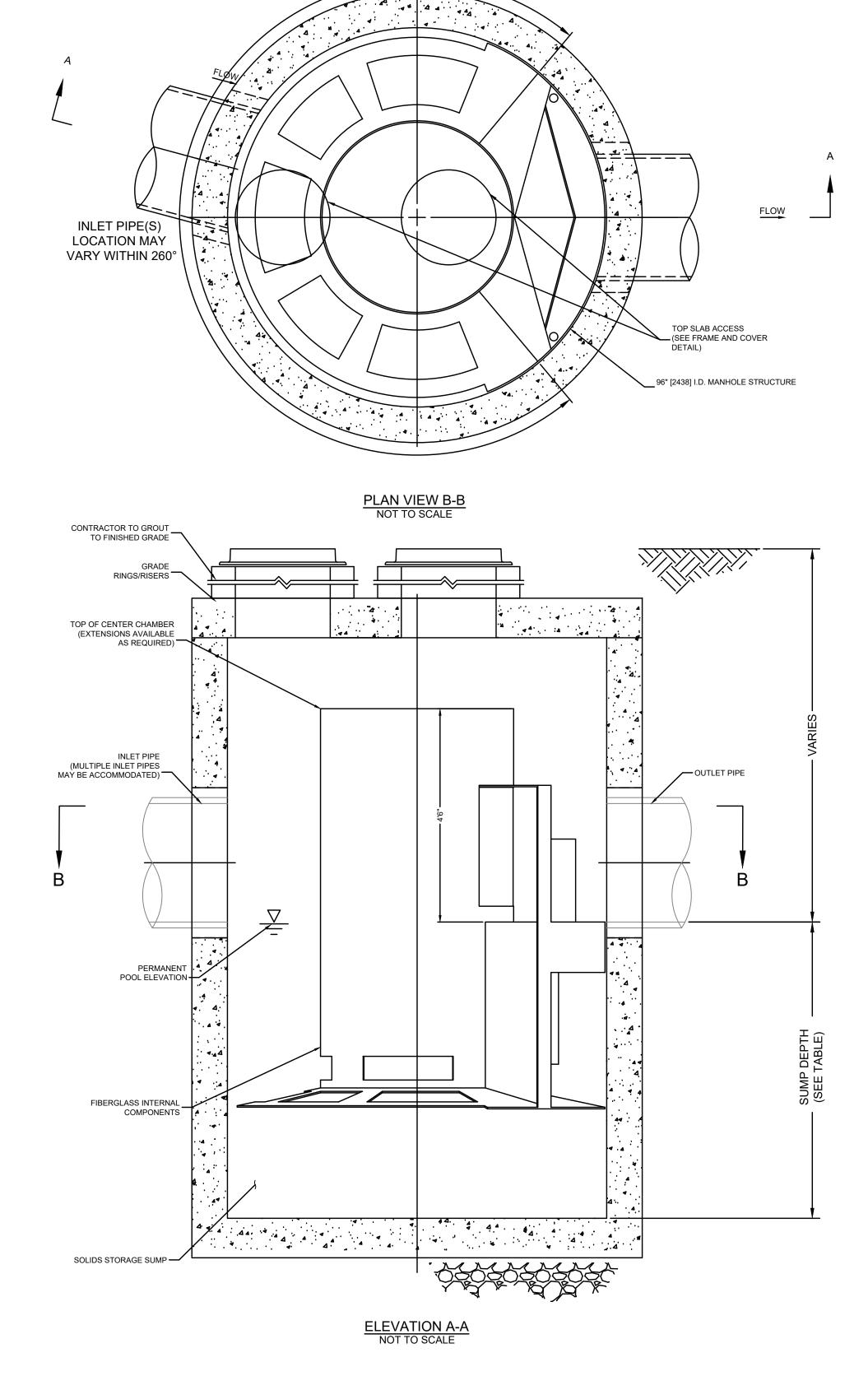
TYPE E CATCH BASIN FRAME

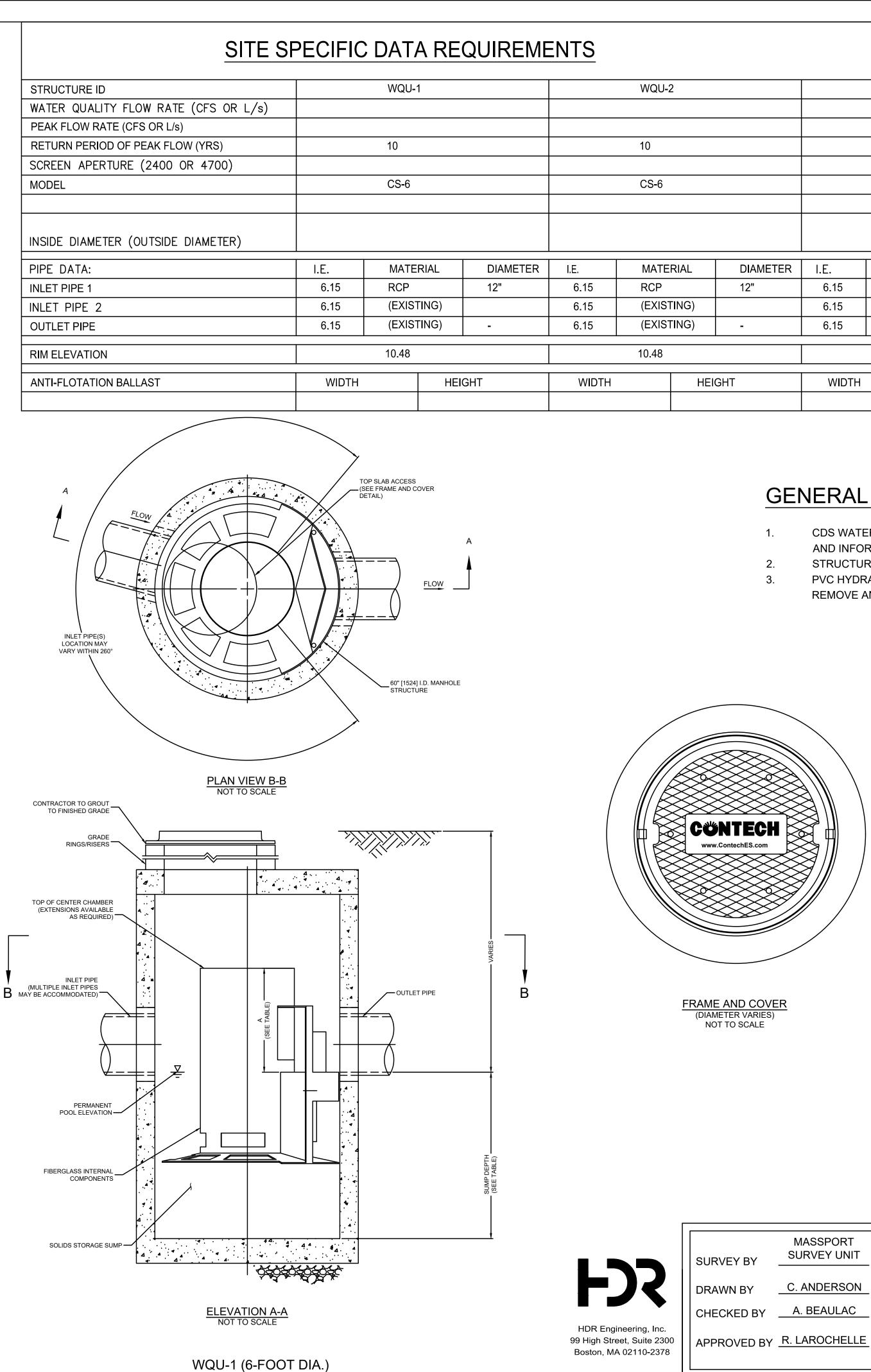
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WQU-2 (8-FOOT DIA.)





		WQU-3				WQU-4			
			10				10		
10						10			
		CS-6				CS-6			
	DIAMETER	I.E.	MATE	RIAL	DIAMETER	I.E.	MATE	RIAL	DIAMETER
	12"	6.15	RCP		12"	6.11	RCP		12"
		6.15	(EXISTING) (EXISTING)			6.11	(EXIS ⁻	ΓING)	
	-	6.15			-	6.11	(EXIS	ΓING)	-
		9.87				9.87			
IEIGHT		WIDTH		HEIGHT		WIDTH		HEIGHT	

GENERAL NOTES:

- CDS WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
- STRUCTURE AND CASTINGS SHALL BE CAST IRON
- PVC HYDRAULIC SHEAR PLATE IS PLACED ON SHELF AT BOTTOM OF SCREEN CYLINDER, REMOVE AND REPLACE AS NECESSARY DURING MAINTENANCE CLEANING.

REVIEWED FOR GENERAL DESIGN AND CONFORMITY TO BWSC STANDARDS

BOSTON WATER AND SEWER COMMISSION

NOT FOR CONSTRUCTION SHEET 16 OF XX

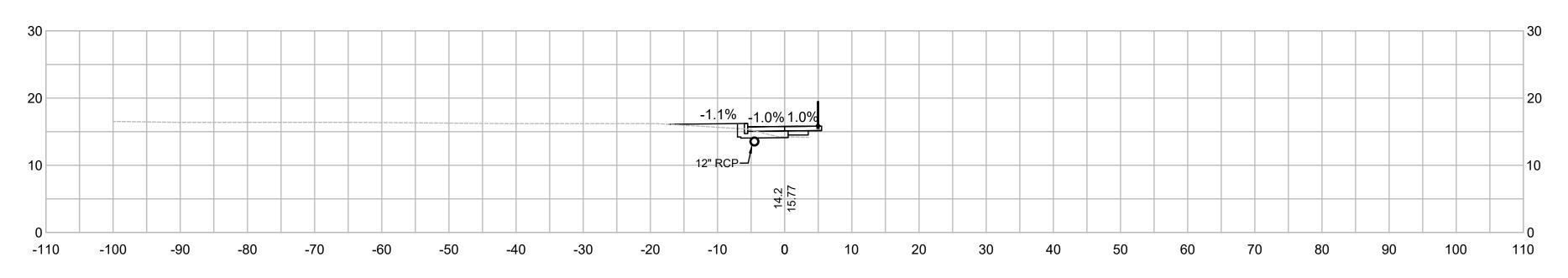
CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

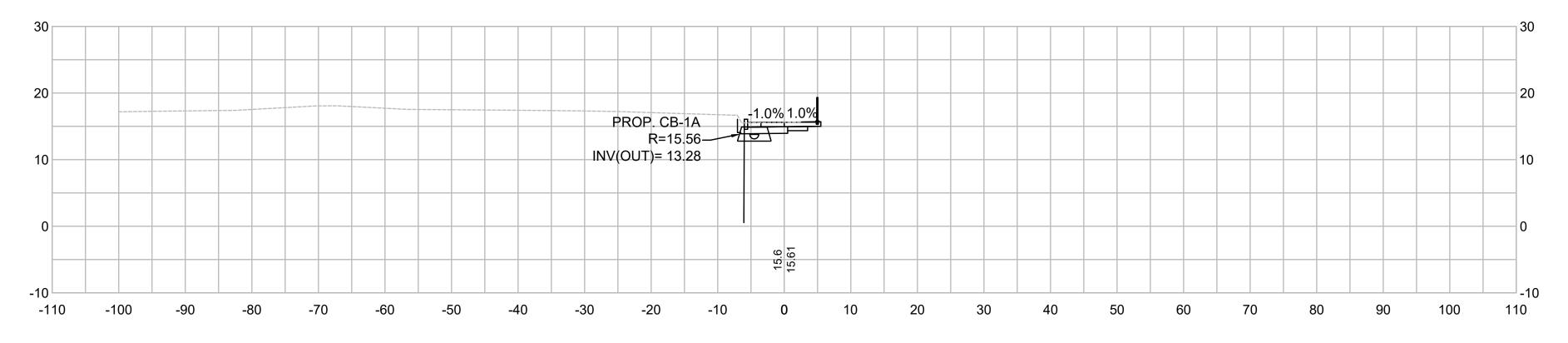
UTILITY DETAILS - 1 CHARLESTOWN

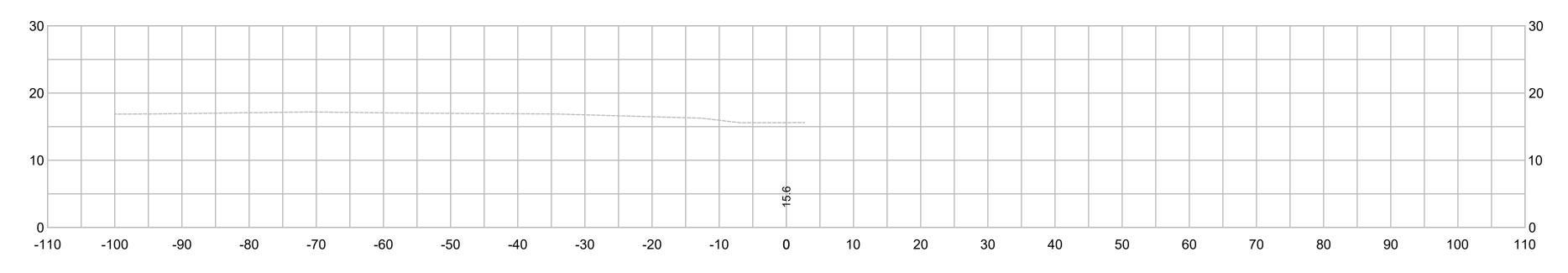
NOT TO SCALE SCALE: DISTRICT:

CITY ENGINEER

WARD 2 DATE: DEC 2024







<u>SCALE:</u> 1" = 20'



101+50

101+00

100+50



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BOSTON PUBLIC WORKS DEPARTMENT

CITY ENGINEER

PART 1 OF 8 PARTS

SURVEY BY

C. ANDERSON DRAWN BY

A. BEAULAC CHECKED BY

APPROVED BY R. LAROCHELLE

SCALE: 1 IN. = 20 FT. DISTRICT: 1

WARD 2 DATE: DEC 2024

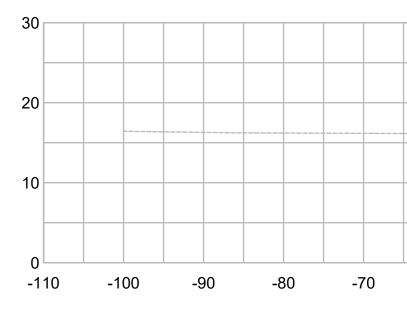
ENGINEERING DIVISION HIGHWAY RECONSTRUCTION SECTIONS

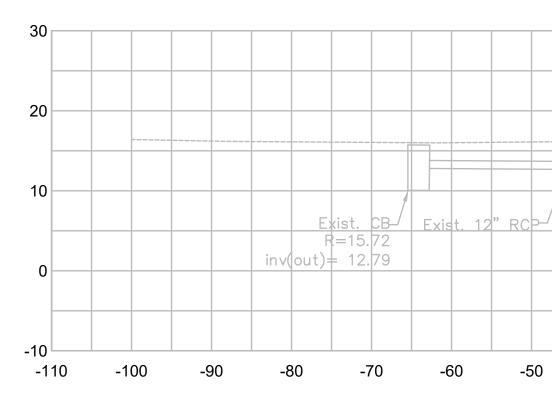
CHARLESTOWN

SHEET 17 OF XX

CITY OF BOSTON PUBLIC WORKS DEPARTMENT

NOT FOR CONSTRUCTION

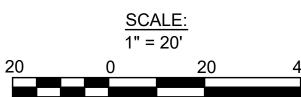












REVIEWED FOR GENERAL DESIGN AND CONFORMITY TO CITY STANDARDS

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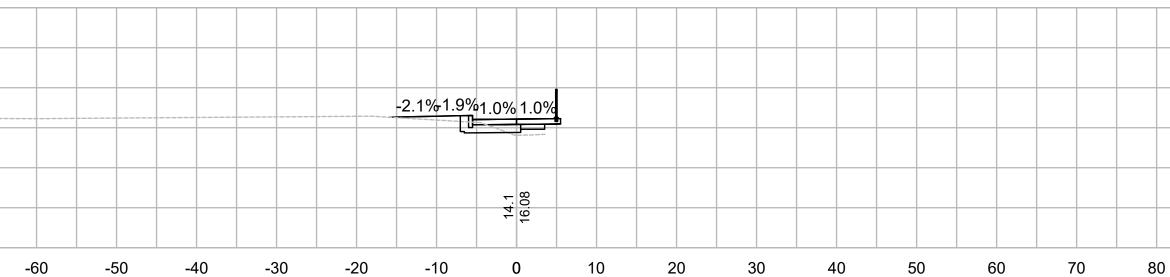


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HDR Engineering, Inc. 99 High Street, Suite 2300 Boston, MA 02110-2378



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SURVEY BY C. ANDERSON A. BEAULAC APPROVED BY R. LAROCHELLE

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NOT FOR CONSTRUCTION

CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

SECTIONS

CHARLESTOWN

SHEET 18 OF XX

WARD 2 DATE: DEC 2024

PART 2 OF 8 PARTS

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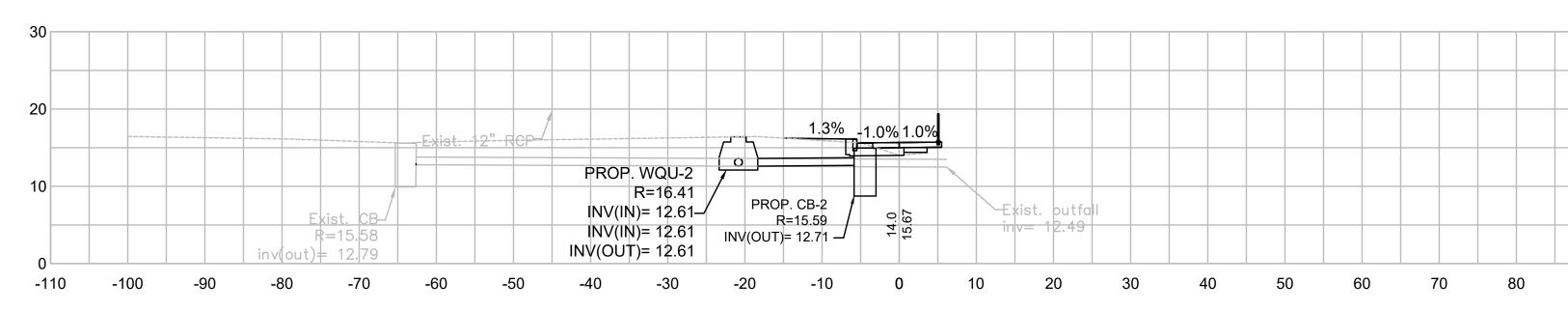
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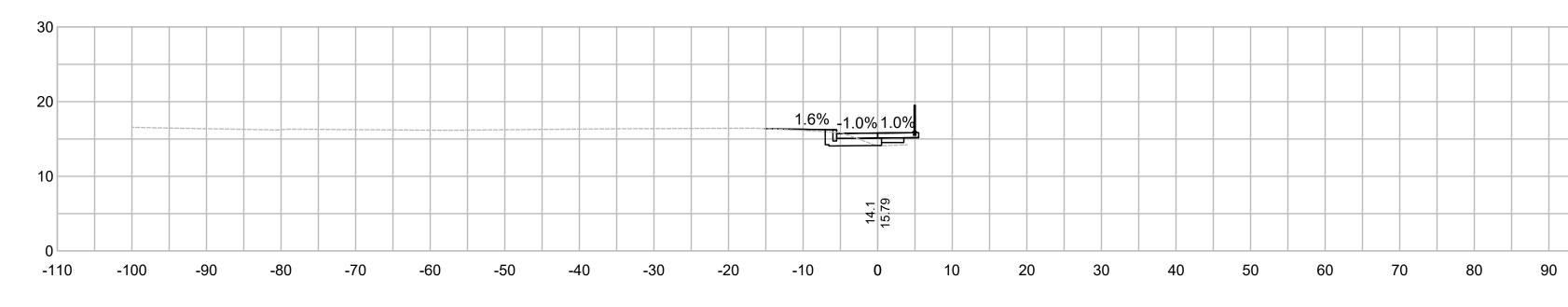
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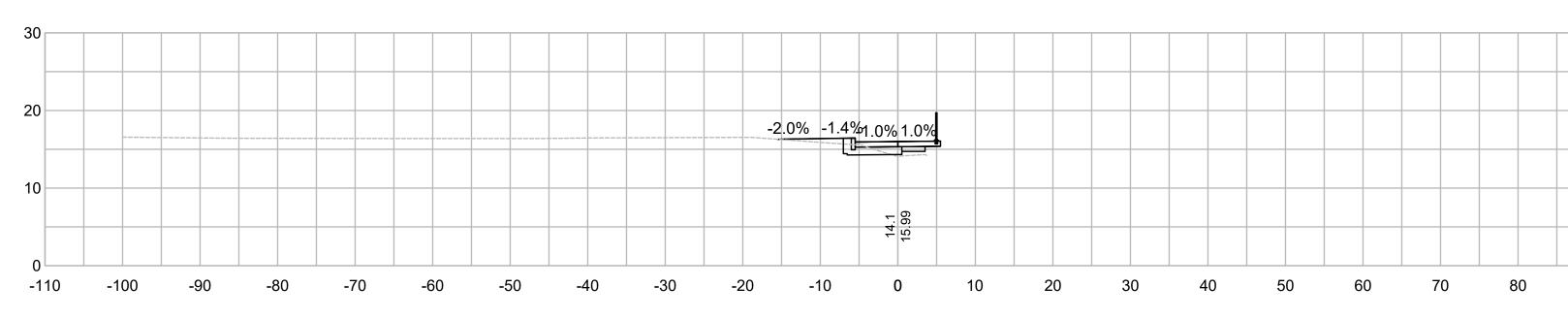
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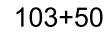
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Boston, MA 02110-2378

REVIEWED FOR GENERAL DESIGN AND CONFORMITY TO CITY STANDARDS

BOSTON PUBLIC WORKS DEPARTMENT

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NOT FOR CONSTRUCTION SHEET 19 OF XX

CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

> SECTIONS CHARLESTOWN

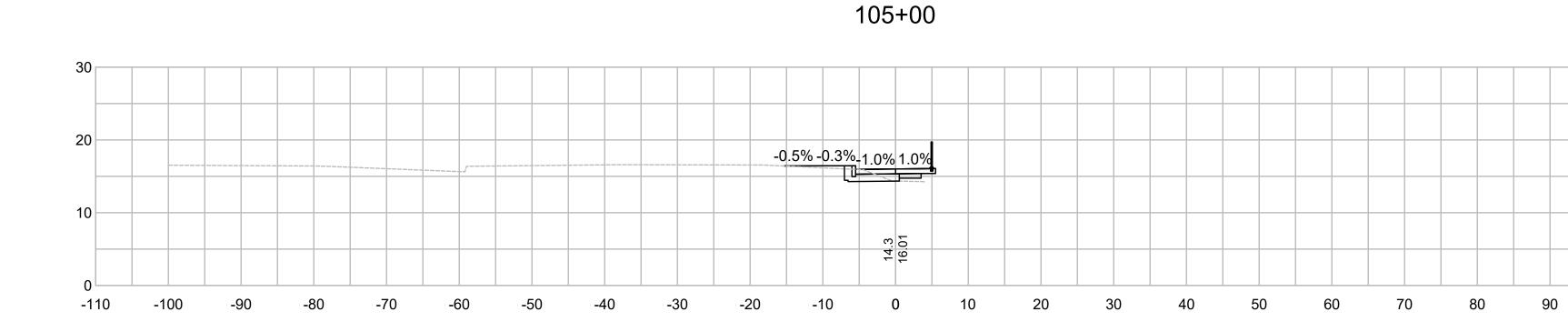
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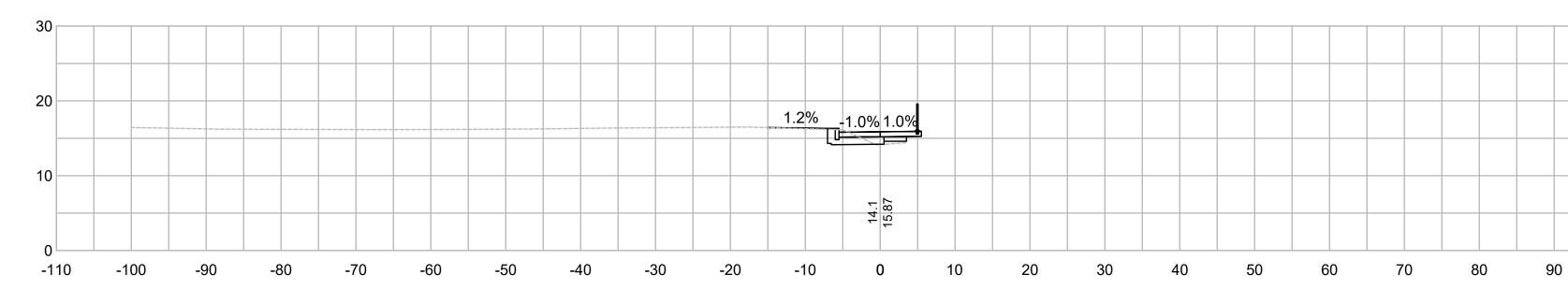
DATE: DEC 2024

SCALE: 1 IN. = 20 FT.

CITY ENGINEER

DISTRICT: 1









HDR Engineering, Inc. 99 High Street, Suite 2300 Boston, MA 02110-2378

REVIEWED FOR GENERAL DESIGN AND CONFORMITY TO CITY STANDARDS

BOSTON PUBLIC WORKS DEPARTMENT



PART 4 OF 8 PARTS				
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DRAWN BY	C. ANDERSON			
CHECKED BY	A. BEAULAC			
APPROVED BY	R. LAROCHELLE			

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NOT FOR CONSTRUCTION SHEET 20 OF XX

CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

SECTIONS

CHARLESTOWN

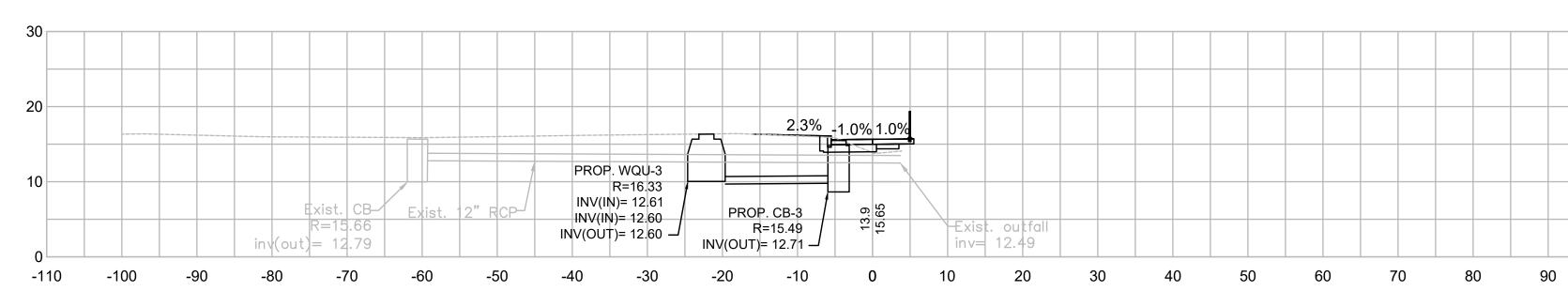
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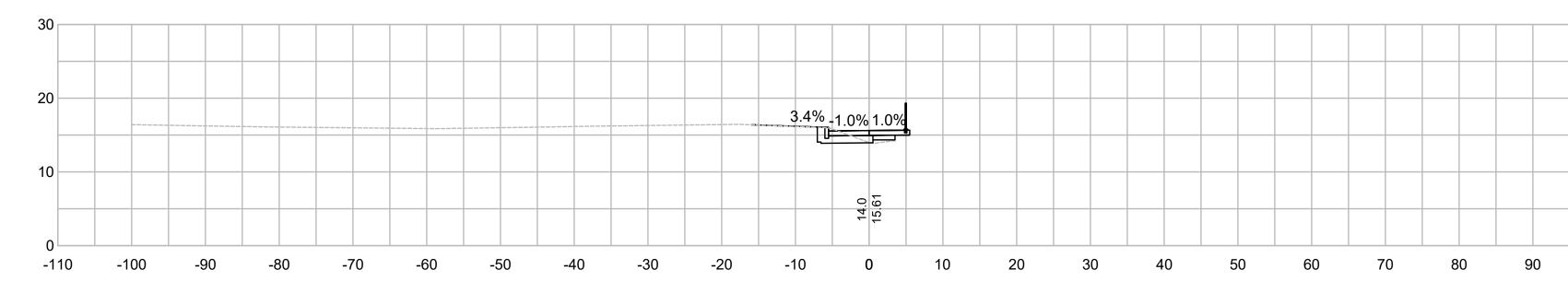
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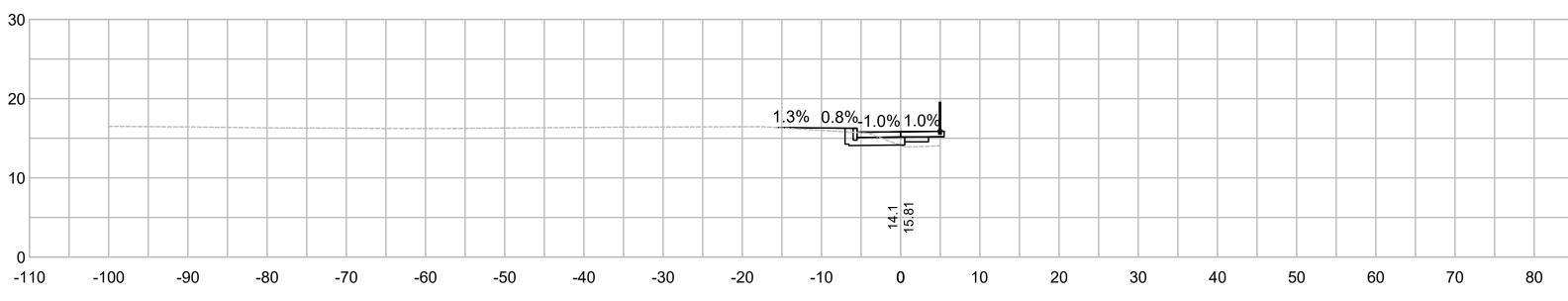
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CITY ENGINEER

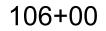
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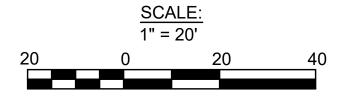


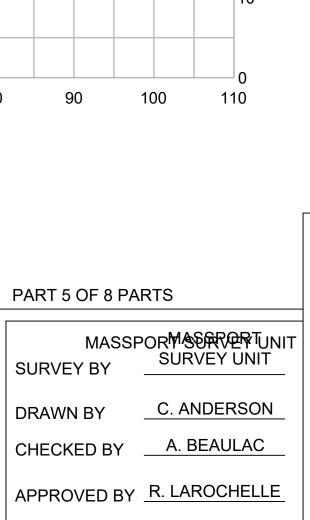
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REVIEWED FOR GENERAL DESIGN AND CONFORMITY TO CITY STANDARDS

BOSTON PUBLIC WORKS DEPARTMENT





NOT FOR CONSTRUCTION SHEET 21 OF XX

CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

SECTIONS

SCALE: 1 IN. = 20 FT.

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CITY ENGINEER

DISTRICT:

CHARLESTOWN

WARD 2 DATE: DEC 2024

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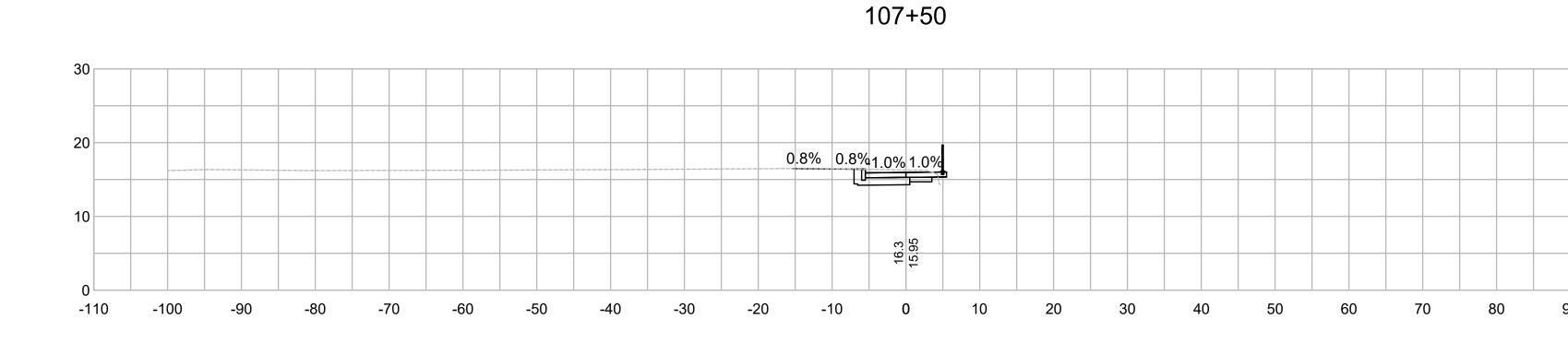
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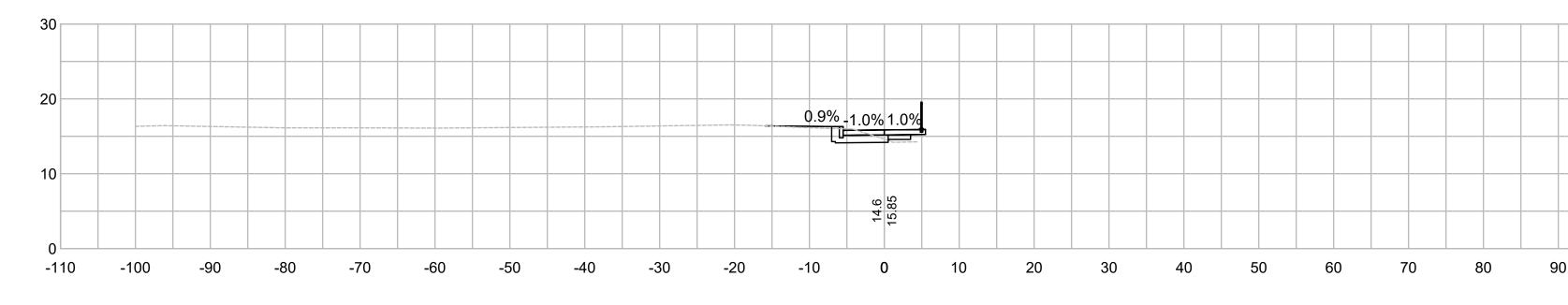
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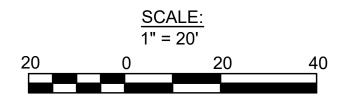




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REVIEWED FOR GENERAL DESIGN AND CONFORMITY TO CITY STANDARDS

BOSTON PUBLIC WORKS DEPARTMENT



PART 6 OF 8 PARTS				
MASSF SURVEY BY	ORMSSRVPRTUNIT SURVEY UNIT			
DRAWN BY	C. ANDERSON			
CHECKED BY	A. BEAULAC			
APPROVED BY	R. LAROCHELLE			

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CITY ENGINEER

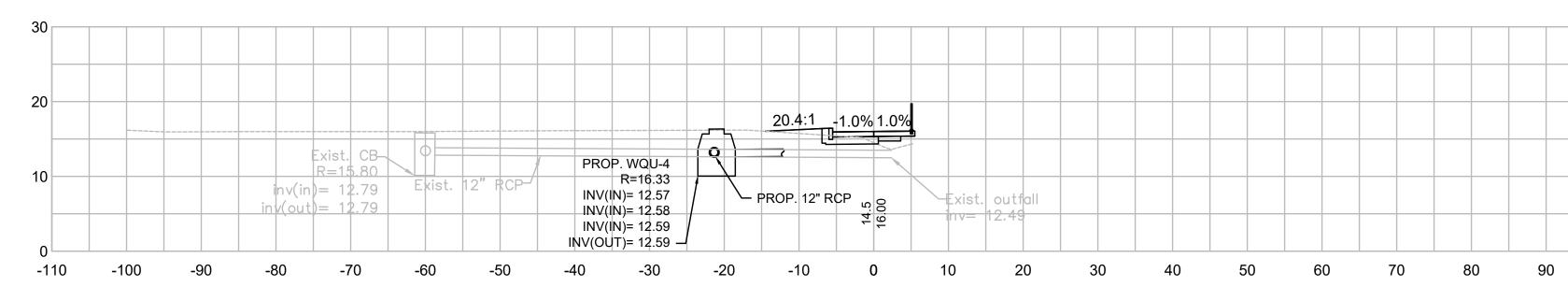
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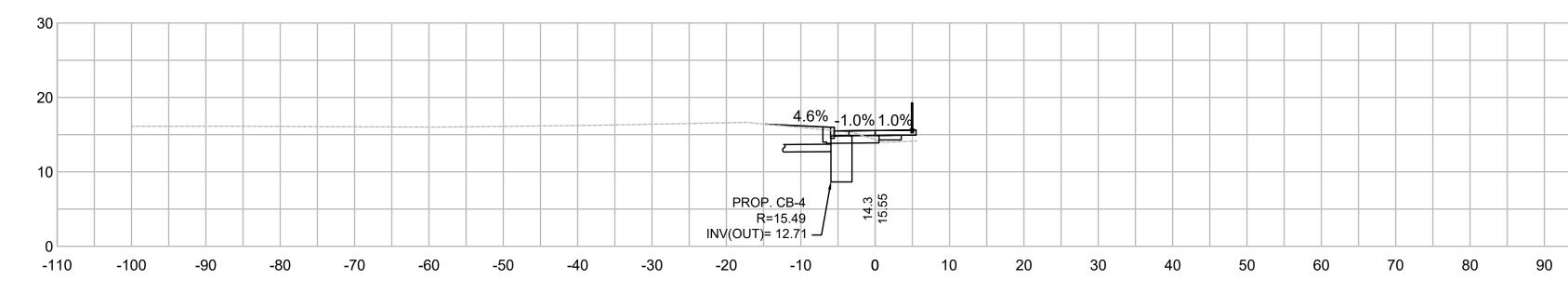
SECTIONS CHARLESTOWN

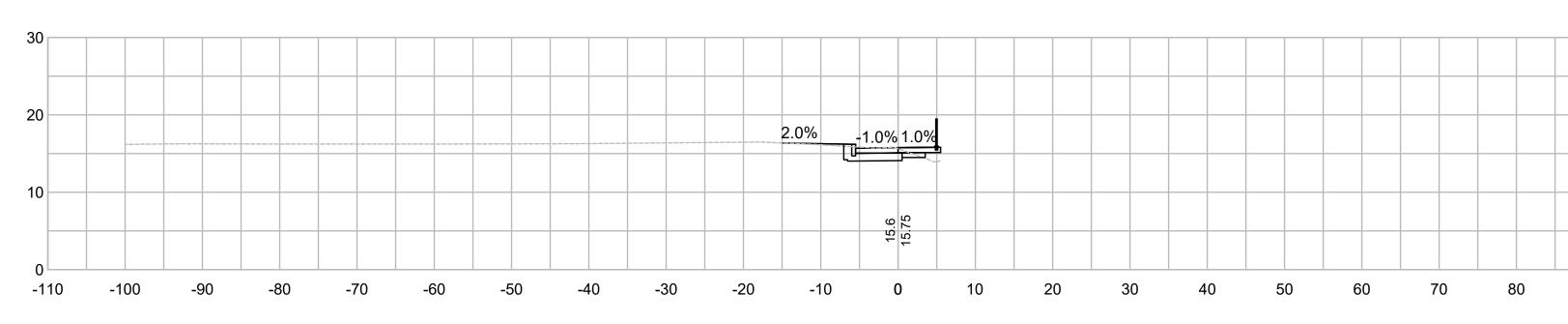
ENGINEERING DIVISION HIGHWAY RECONSTRUCTION

CITY OF BOSTON PUBLIC WORKS DEPARTMENT

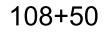
NOT FOR CONSTRUCTION SHEET 22 OF XX









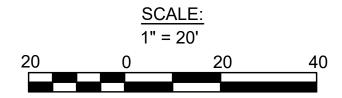


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REVIEWED FOR GENERAL DESIGN AND CONFORMITY TO CITY STANDARDS

BOSTON PUBLIC WORKS DEPARTMENT



	PART 7 OF 8 PA	RTS
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CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION

HIGHWAY RECONSTRUCTION

CHARLESTOWN

WARD 2

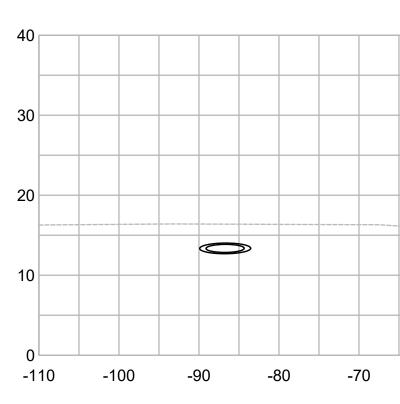
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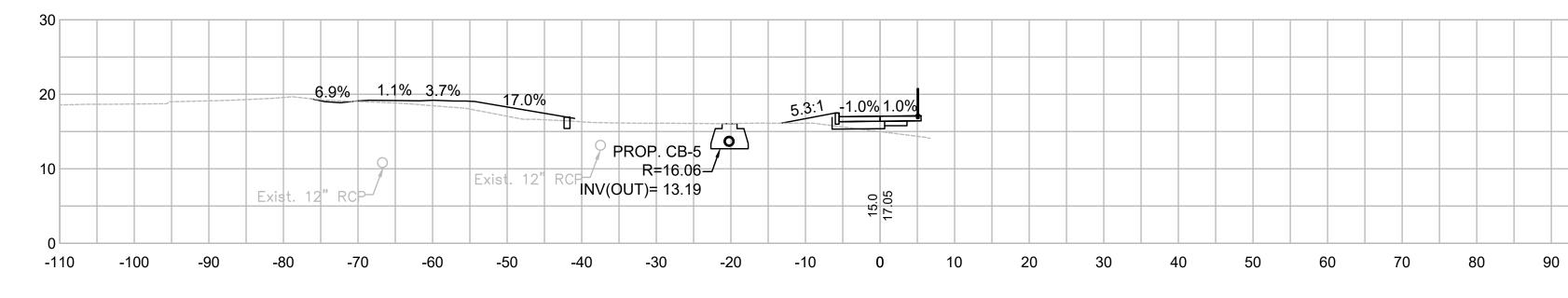
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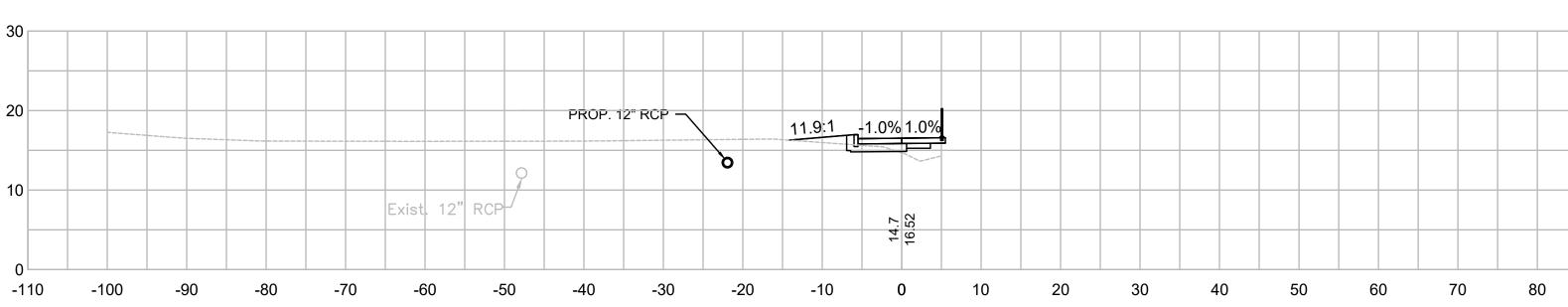
CITY ENGINEER

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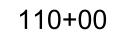








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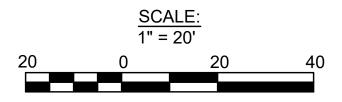
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HDR Engineering, Inc. 99 High Street, Suite 2300 Boston, MA 02110-2378

REVIEWED FOR GENERAL DESIGN AND CONFORMITY TO CITY STANDARDS

BOSTON PUBLIC WORKS DEPARTMENT



90 100 110 PART 8 OF 8 PARTS SURVEY BY C. ANDERSON DRAWN BY A. BEAULAC CHECKED BY APPROVED BY R. LAROCHELLE

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CITY ENGINEER

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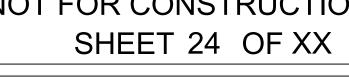
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
WARD 2 DATE: DEC 2024

HIGHWAY RECONSTRUCTION SECTIONS CHARLESTOWN

NOT FOR CONSTRUCTION SHEET 24 OF XX

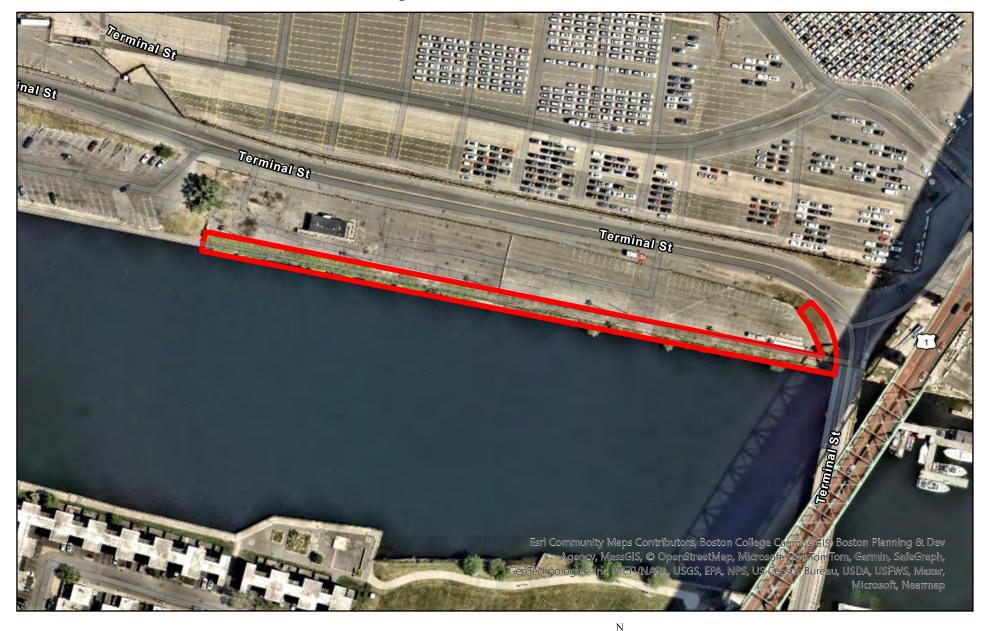
CITY OF BOSTON PUBLIC WORKS DEPARTMENT ENGINEERING DIVISION

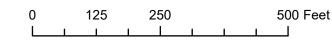






Harborwalk Extension - Project Extents





EXISTING CONNECTIVITY ISSUES

Existing Harborwalk

BTD Intersection Project

Proposed Project Site

NO SIDEWALK

IT.E.E.

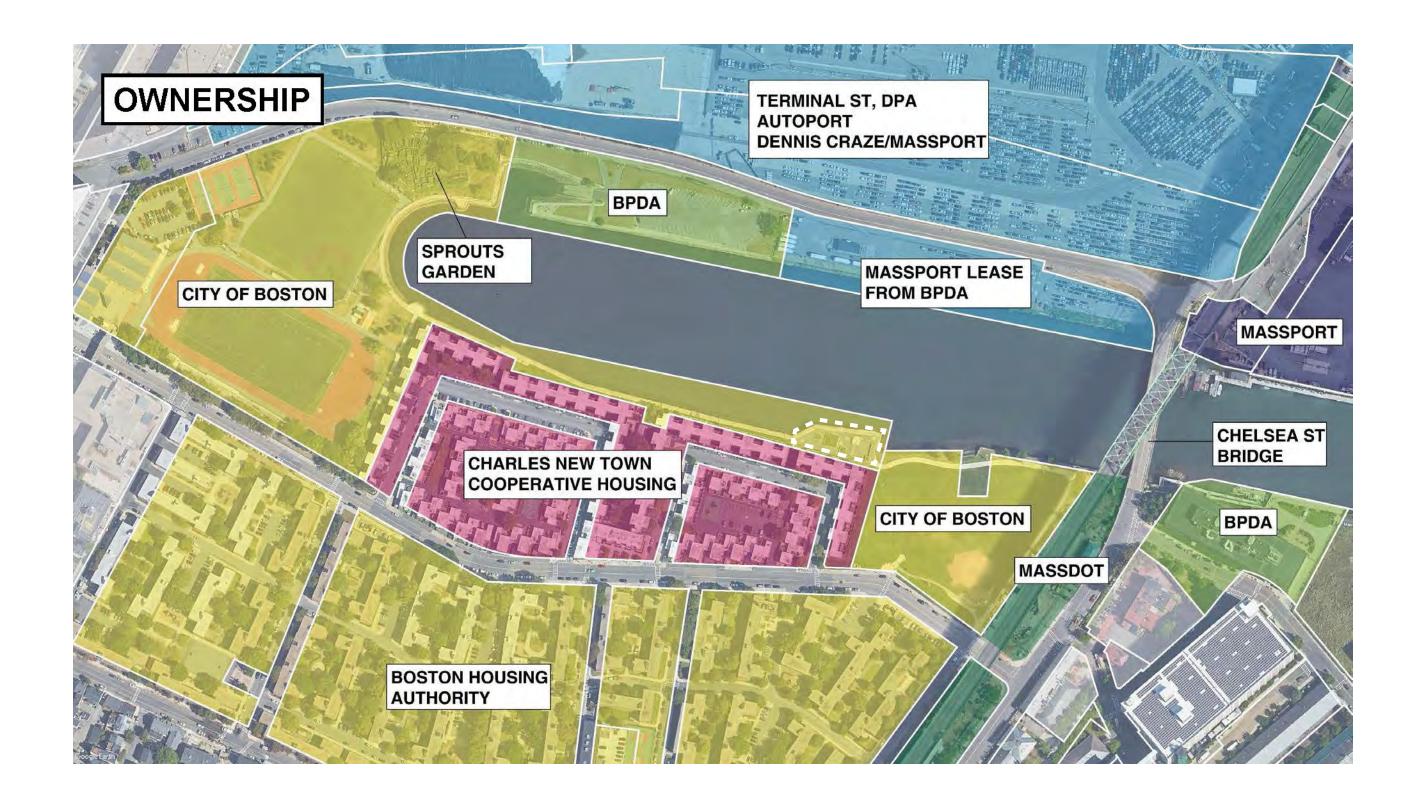
PATHWAY ENDS

CHELSEA ST.

EXISTING SIDEWALK

Existing Harborwalk

Barry Field Connector Ramp





The Commonwealth of Massachusetts MASSACHUSETTS SENATE

Senator Sal DiDomenico ASSISTANT MAJORITY LEADER Middlesex and Suffolk District

State House, Room 405 Boston, MA 02133-1053 Tel. (617) 722-1650 Fax (617) 722-1323 Sal.DiDomenico@MAsenate.gov www.MAsenate.gov *Chair* Bills in the Third Reading

Vice Chair Education Steering and Policy Labor and Workforce Development Personnel and Administration Export Development

January 22, 2025 Amanda Lewis MassTrails Program Manager 136 Damon Road Northampton, MA 01060

Dear Ms. Lewis,

I am writing in support of the Harborwalk Extension Project, a City of Boston project located in the Little Mystic Channel area of Charlestown that will be shovel ready in 2025. This project will advance the Charlestown communities goals related to increasing mobility in the area and building climate resiliency.

The project site is currently underutilized, existing as the overgrown edge of a parcel leased for single occupancy vehicle storage. The proposed construction project would fill a much-needed gap in the Boston Harborwalk, a 43-mile linear park along Boston's shoreline, and the Mystic Greenways, 25-miles of safe and accessible routes for walking, biking and rolling from the Mystic Lakes to Boston Harbor. The project will compliment other complete streets projects connecting to the project site, helping to offset traffic congestion and provide safe accommodations for active mobility in a census block that is in the 99th state percentile for proximity to traffic and diesel particulate matter (EPA 2025). The project would provide a meaningful opportunity for social and recreational engagement for neighboring residents and bring much-needed investment to an MA Office of Energy and Environmental Affairs -designated Environmental Justice census block comprised of 76% low-income households, 79% people of color, and a \$13,004 average per-capita income (EPA 2025).

I urge the Massachusetts Gaming Commission to prioritize funding for this critical project. Please feel free to contact my office with any questions.

State Senator Middlesex & Suffolk



DANIEL J. RYAN

STATE REPRESENTATIVE

2ND SUFFOLK DISTRICT

The Commonwealth of Massachusetts

HOUSE OF REPRESENTATIVES STATE HOUSE, BOSTON 02133-1020

> CHAIR: Joint Committee on Election Laws

STATE HOUSE, ROOM 445 TEL. (617) 722-2460 Dan.Ryan@MAhouse.gov

January 22, 2025

Amanda Lewis MassTrails Program Manager 136 Damon Road Northampton, MA 01060

Dear Ms. Lewis,

I write today in strong support of the Harborwalk Extension Project, a City of Boston project located in the Little Mystic Channel area of Charlestown. This initiative will bring tremendous benefit toward the advancement of our community's goals for mobility and climate resiliency.

The project site currently exists as an overgrown edge of a parcel leased for single occupancy vehicle storage. The proposed construction project would fill a much-needed gap along the Boston Harborwalk, a 43-mile linear park along Boston's shoreline, and the Mystic Greenways, 25-miles of safe and accessible routes for walking, biking and rolling from the Mystic Lakes to Boston Harbor. It will compliment other ongoing complete streets initiatives, which connect to the site, helping to offset traffic congestion and provide safe accommodations for active mobility. This venture also provides a meaningful opportunity for social and recreational engagement for neighboring residents, while bringing much-needed investment to an MA EEA-designated EJ census block comprised of 76% low-income households, 79% people of color, and a \$13,004 average per-capita income (EPA 2025).

The Little Mystic Channel area is actively used by community members and visitors daily. In addition to its location on the Boston Harborwalk and the Mystic Greenways, the channel hosts a community garden, public boat ramps, community sports fields and the East Coast Greenway, a national bike route spurring economic development and tourism opportunities.

Thank you in advance for your consideration, and I ask that you look kindly on their application. If you should wish to discuss further, please contact me at your convenience.

Daniel Ryan

State Representative



Massachusetts Port Authority One Harborside Drive, Suite 200S East Boston, MA 02128-2909 Telephone (617) 568-5000 www.massport.com

January 24, 2025

Amanda Lewis MassTrails Program Manager 136 Damon Road Northampton, MA 01060

Dear Ms. Lewis,

I am writing on behalf of the Massachusetts Port Authority ("Massport") to express support for the City of Boston's Harborwalk Extension Project along the Little Mystic Channel in Charlestown. The Harborwalk Extension Project is consistent with broader community, waterfront access, and transportation goals in this area and represents an important contribution to neighborhood.

Massport controls dozens of acres of land along Terminal Street in Charlestown, which are home to the Boston Autoport and many other maritime industrial businesses. The Autoport and its subtenants are important to the Port of Boston, with activities ranging from importing automobiles and road salt to hosting the Massachusetts Clean Energy Center's Wind Technology Testing Center. The Autoport land and the project site are located within the Mystic River Designated Port Area ("DPA").

The project will remove a strip of land not suitable for maritime industrial operations from the Boston Autoport's lease area and create an extension of the Harborwalk along the water's edge. This new section of Harborwalk will fill a missing gap and create a continuous pedestrian access loop around the Little Mystic Channel. This new connection will help reduce pedestrians on Terminal Street, an important truck route along which there is no existing sidewalk. By directing the public to the new Harborwalk and away from Terminal Street, the project will create a more pleasant pedestrian experience and minimize potential conflicts. As part of a \$2 million contribution to Charlestown community benefits, Massport recently funded and constructed a similar pedestrian access improvement project on the southern side of the Little Mystic in the form of a ramp/stair structure also intended to improve the Harborwalk and pedestrian safety.

Massport staff has been engaged with the City of Boston and the Mystic River Watershed Association on this plan, and we appreciate the team's consideration of the needs of the Autoport Amanda Lewis January 24, 2025 Page 2

and the maritime industrial users within the Mystic River DPA. In particular, the team has worked effectively with Massport's Autoport tenant to understand and address their concerns and needs. The project does not appear to be detrimental to maritime industrial operations in the area, and it represents an opportunity to educate the public about the activities and businesses at the Autoport and the broader role and contributions of the Port of Boston through information panels or other interpretive installations. We support this project and its request for funding.

Hargh

Andrew G. Hargens, AICP | LEED AP Chief Development Officer Massachusetts Port Authority



GABRIELA COLETTA ZAPATA DISTRICT 1 BOSTON CITY COUNCILOR

January 15, 2025

Mary Thurlow Massachusetts Gaming Commission 101 Federal St., 12 th Floor Boston, MA 02110

RE: Boston Harborwalk Extension

Dear Ms. Thurlow,

I am writing in strong support of the Harborwalk Extension Project, a City of Boston project located in the Little Mystic Channel area of Charlestown that will be shovel ready in 2025. As the City Councilor representing Charlestown, I am very excited about this project as it will bring tremendous benefit toward the advancement of our community's goals for mobility and climate resiliency.

The project site, currently undervalued and used for single-occupancy vehicle storage, presents a transformative opportunity. It would bridge a critical gap in the Boston Harborwalk—a 43-mile shoreline park—and the Mystic Greenways, 25 miles of accessible routes for walking, biking, and rolling. The project will complement other complete street projects connecting to the project site, helping offset traffic congestion and providing safe accommodations for active mobility in a census block in the 99th state percentile for proximity to traffic and diesel particulate matter (EPA 2025). The project would also provide social and recreational opportunities, bringing essential investment to a designated environmental justice census block with 76% low-income households, 79% people of color, and a \$13,004 average per-capita income (EPA 2025).

The Little Mystic Channel area is a long-neglected area of Charlestown that thousands of community members and visitors use daily. In addition to its location on the Boston Harborwalk and the Mystic Greenways, the channel hosts a community garden, public boat ramps, community sports fields, and the East Coast Greenway, a national bike route spurring economic development and tourism opportunities. On any given day, you can find the area around the

Channel filled with people fishing, gardening, playing softball, taking an evening stroll, biking, or launching a boat. Ensuring safe access to, from, and through this diamond-in-the-rough destination is a priority for my District. Please feel free to contact me at <u>gabriela.coletta@boston.gov</u> or (617) 635-3200 with any questions.

Sincerely,

Galreele Colette Zapata

Gabriela Coletta Zapata Boston City Councilor, District 1 East Boston, Charlestown, North End, and Wharf District



Amanda Lewis MassTrails Program Manager 136 Damon Road Northampton, MA 01060

Re: Support for Boston Harborwalk Extension Construction

Dear Ms. Lewis,

I'm writing to you on behalf of the Mystic River Watershed Association (MyRWA), whose mission is to protect and restore the Mystic River and its tributaries. Our vision is a healthy, vibrant, and resilient Mystic River Watershed for the benefit of all our community members. MyRWA has worked with thousands of residents in Charlestown to protect water quality, restore important habitats, build climate resilience, transform parks and paths, inspire youth and grow community. Over the past decade, we have advocated for, and took action on, revitalizing the local environment through education, trash removal, water quality improvement, and the management of the Mystic Greenways Program vision – 25 miles of connected paths and parks from the Mystic Lakes to Boston Harbor.

MyRWA strongly supports the City of Boston's application for the construction of the Boston Harborwalk Extension Project in the Little Mystic Channel area of Charlestown. The project site is currently undervalued and of little use to the Boston community. It will fill a much-needed gap in the Boston Harborwalk and will compliment other complete streets projects connecting to the project site, helping to offset traffic congestion and provide safe accommodations for active mobility in a census block that is in the 99th state percentile for proximity to traffic and exposure to diesel particulate matter (EPA 2025). It will provide a meaningful opportunity for social and recreational engagement for neighboring residents and bring much-needed investment to an MA EEAdesignated EJ census block comprised of 76% low-income households, 79% people of color, and a \$13,004 average per-capita income (EPA 2025).

Since 2019, we have supported comprehensive, community-driven planning and design for a revitalized Little Mystic Channel, working with community members to optimize the project's impact on local residents in the adjacent affordable housing, as well as the larger region. On any given day, you can find the area around the Channel filled with people fishing, gardening, playing softball, taking an evening stroll, biking, or launching a boat. Ensuring safe access to, from and through this diamond-in-the-rough destination is a core priority for our organization.

Patrick the Heurt

Patrick Herron Executive Director Mystic River Watershed Association

January 8, 2025

Amanda Lewis MassTrails Program Manager 136 Damon Road Northampton, MA 01060

Dear Ms. Lewis,

On behalf of the Charlestown Little Mystic Channel Steering Committee, I am writing in strong support of the Harborwalk Extension Project, a City of Boston project located in the Little Mystic Channel area of Charlestown that will be shovel ready in 2025. The Little Mystic Steering Committee is a community-led group of 9 diverse Charlestown residents working to improve the area around the Little Mystic with the guiding principles of equity, engagement, and climate resilience. We aim to engage neighboring residents and the entire Charlestown community on working to improve the Little Mystic Channel. The steering committee is very excited about this project as it will bring tremendous benefit to residents and would be a source of pride for the community.

The project site is currently undervalued and of little use to the Charlestown community. It will fill a much-needed gap in the Boston Harborwalk, a 43-mile linear park along Boston's shoreline, and the Mystic Greenways, 25-miles of safe and accessible routes for walking, biking and rolling from the Mystic Lakes to Boston Harbor. The project will compliment other complete streets projects connecting to the project site, helping to offset traffic congestion and provide safe accommodations for active mobility in a census block that is in the 99th state percentile for proximity to traffic and diesel particulate matter (EPA 2025). It will provide a meaningful opportunity for social and recreational engagement for neighboring residents and bring much-needed investment to an MA EEA-designated EJ census block comprised of 76% low-income households, 79% people of color, and a \$13,004 average per-capita income (EPA 2025).

The Little Mystic Channel area is a long-neglected area of Charlestown that is used by thousands of community members and visitors every day. In addition to its location on the Boston Harborwalk and the Mystic Greenways, the channel hosts a community garden, public boat ramps, community sports fields and the East Coast Greenway, a national bike route spurring economic development and tourism opportunities. On any given day, you can find the area around the Channel filled with people fishing, gardening, playing softball, taking an evening stroll, biking, or launching a boat. Ensuring safe access to, from and through this diamond-in-the-rough destination is a core priority for the community.

Victor M. Castro On behalf of the Charlestown Little Mystic Steering Committee

January 21, 2025



Board of Trustees

Chair: Regina Yan, CA Vice Chair: Atiba Mbiwan, GA Treasurer: Spencer Finch, NC Secretary: Lori Bush, NC Ben Dalley, DC Claire Grega, PA Joseph Impellizeri, MA Becky Smith, DC Nicole Thompson, NC

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Executive Director Dennis Markatos-Soriano Amanda Lewis MassTrails Program Manager 136 Damon Road Northampton, MA 01060

Dear Ms. Lewis,

On behalf of the East Coast Greenway Alliance ("ECGA"), we are writing in strong support of the Harborwalk Extension Project, a City of Boston project located in the Little Mystic Channel area of Charlestown that will be shovel ready in 2025. ECGA partners with local, state, and national agencies and organizations to promote the establishment, stewardship, and public enjoyment of a safe and accessible multi-user greenway linking cities and towns from Maine to Florida. The East Coast Greenway travels along the Little Mystic Channel, and we are very excited about this project as it will bring tremendous benefit to Boston residents and visitors to the region.

The project site is currently undervalued and of little use to the Boston community. It will fill a much-needed gap in the Border to Boston Trail, the Boston Harborwalk, and also the Mystic Greenways - 25-miles of safe and accessible routes for walking, biking and rolling from the Mystic Lakes to Boston Harbor. The project will compliment other complete streets projects connecting to the project site, helping to offset traffic congestion and provide safe accommodations for active mobility in a census block that is in the 99th state percentile for proximity to traffic and diesel particulate matter (EPA 2025). It will provide a meaningful opportunity for social and recreational engagement for neighboring residents and bring much-needed investment to an MA EEA-designated EJ census block comprised of 76% low-income households, 79% people of color, and a \$13,004 average per-capita income (EPA 2025).

The Little Mystic Channel area is a long-neglected area of Charlestown that is used by thousands of community members and visitors every day. In addition to its location on the East Coast Greenway, Border to Boston Trail, Boston Harborwalk and the Mystic Greenways, the channel hosts a community garden, public boat ramps, and community sports fields. On any given day, you can find the area around the Channel filled with people fishing, gardening, playing softball, taking an evening stroll, biking, or launching a boat. Ensuring safe access to, from, and through this diamond-in-the-rough destination is a core priority for our organization.

De Ma

Dennis Markatos-Soriano Executive Director

Emily Pastervicey

Emily Paskewicz Northern New England Manager





January 8, 2025

Amanda Lewis MassTrails Program Manager 136 Damon Road Northampton, MA 01060

Dear Ms. Lewis,

On behalf of Boston Harbor Now ("BHN") and the Friends of the Boston Harborwalk ("FoBH"), we are writing in strong support of the Harborwalk Extension Project, a City of Boston project located in the Little Mystic Channel area of Charlestown that will be shovel ready in 2025. For over three decades, BHN and FoBH have worked closely with the City of Boston, state agencies, private developers and waterfront residents to advocate for, activate, and protect a 43-mile linear park along Boston's shoreline. We are very excited about this project as it will bring tremendous benefit to Boston residents and would be a source of pride for the community.

The project site is currently undervalued and of little use to the Boston community. It will fill a much-needed gap in the Boston Harborwalk and also the Mystic Greenways, 25-miles of safe and accessible routes for walking, biking and rolling from the Mystic Lakes to Boston Harbor. The project will compliment other complete streets projects connecting to the project site, helping to offset traffic congestion and provide safe accommodations for active mobility in a census block that is in the 99th state percentile for proximity to traffic and diesel particulate matter (EPA 2025). It will provide a meaningful opportunity for social and recreational engagement for neighboring residents and bring much-needed investment to an MA EEA-designated EJ census block comprised of 76% low-income households, 79% people of color, and a \$13,004 average percapita income (EPA 2025).

The Little Mystic Channel area is a long-neglected area of Charlestown that is used by thousands of community members and visitors every day. In addition to its location on the Boston Harborwalk and the Mystic Greenways, the channel hosts a community garden, public boat ramps, community sports fields and the East Coast Greenway, a national bike route spurring economic development and tourism opportunities. On any given day, you can find the area around the Channel filled with people fishing, gardening, playing softball, taking an evening stroll, biking, or launching a boat. Ensuring safe access to the waterfront is a core priority for our organizations, and we see this project as forwarding our vision for a more accessible Boston waterfront.

Kath Alan

President and CEO Boston Harbor Now

Michael Manning

Michael Manning Friends of the Boston Harborwalk

City of Boston's / Boston Police Department's Response to the 2026 Mass Gaming Commission's Block Grant Program

Please use the space below to identify the impact of the gaming establishment on your municipality. You may use the impacts identified in the FY 2026 Guidelines relevant to this category. If you are using an impact not identified in the guidelines, please use the space below to identify the impact. Please provide documentation or evidence that gives support for the determination that the operation of the gaming facility caused or is causing the impact (i.e., surveys, data, reports, etc.)

Identified Impacts: The BPD will address all of the following impacts within our proposed strategies:

- Increased visitation and employment due to the casino will likely increase the interaction between public safety personnel and casino patrons and employees.
- It is recognized by law enforcement and the casino industry that certain types of crime may be attracted to casinos and other hospitality-related businesses. These may include human trafficking, money laundering, and drug trafficking. Other crimes that may be attributable to casinos include increased assaults, fraud, and property crimes.
- The presence of casinos has been demonstrated to cause an increase in cases of operating under the influence.
- Increases in traffic can cause increases in congestion, accidents, and vehicular/bicycle/pedestrian conflicts.
- The influx of visitors to a casino can result in an increase in calls for service and put pressure on local emergency services including emergency responders like police, fire departments and EMS. This could lead to increased needs for mutual aid.

For this section of our response to this question, we will only note two new sources of information, as within the 2025 application, we had already noted several other research summaries.

- 1) During a recent search on Chat GPT (an artificial intelligence app), by one of the Department's District Captains, when prompted with the question, Chat GPT identified the following "Crime Associated with Casino Activities":
 - Money laundering,
 - Fraud and cheating
 - Gambling addiction
 - Drug trafficking
 - Prostitution and human trafficking
 - Loan sharking and extortion
 - Violent crime, and
 - Tax evasion

Each of these crime categories has a negative community impact, including; economic consequences, social consequences, physical and emotional harm, and quality of life consequences.

2) Impacts of casino gambling on public safety Sheila McKnight, Senior Knowledge Broker Lindsay Kalbfleisch, Director of Stakeholder Engagement Sasha Stark, PhD, Director of Research and Evidence Services Presentation of Evidence for Massachusetts Gaming Commission September 2023

Encore Boston Harbor Casino had few direct links with public safety issues during its first year and four months of operation (July 2019 to October 2020). Overall, the region and most cities had expected levels

of crime. In its first six months of operation, there were increases in crime and calls for service that were not unexpected given the number of people who were drawn to the casino (more than 3.5 million visitors during the first eight months). In Everett, not accounting for any surrounding community impact, this resulted in a 9.5% increase in violent crime, a 5.1% increase in property crime, a 6% increase in total crime, and a 4.1% increase in calls for service. In its first six months of operation, Encore Boston Harbor casino itself accounted for 124 arrests, including 22 violent crimes and 20 property crimes. Increases occurred in some public safety issues across the wider region surrounding the casinos in the early months and years they were open. It is unclear if these increases are directly linked to the presence of casinos, unless otherwise indicated. These included drunk driving, vehicle collisions and traffic, property crime, domestic violence, sexual assault, shoplifting, purse snatching, and other crimes in commercial establishments, credit card fraud, money laundering, prostitution, local emergency services and other.

As stated, from the 2025 application, we did a literature review of studies that show the Encore Casino's impact on crime, and other studies that show gaming's influence on crime. This is the literature that we added into the 2025 application. If a summary of these reports is needed within this year's application, please let us know.

- Assessing the Influence of Gambling on Public Safety in Massachusetts Cities and Towns Analysis of the influence of Encore Boston Harbor on its surrounding community during its first six months of operation Christopher W. Bruce Crime Analysis Consultant to the Massachusetts Gaming Commission 10 April 2020 v. 1.5
- CASINOS, CRIME, AND COMMUNITY COSTS Earl L. Grinols and David B. Mustard 2006
- Problem gambling and family violence: family member reports of prevalence, family impacts and family coping Aino Suomi, Alun C Jackson, Nicki A Dowling, Tiffany Lavis, Janet Patford, Shane A Thomas, Peter Harvey, Max Abbott, Maria E Bellringer, Jane Koziol-McLain and Sue Cockman

Boston Crime Incident Data 2024:

- In **District A1**, in 2024, the only Part One property crime category that saw an increase from 2023 was Other Larceny (1,342 to 1,402); while Part One violent crime was only up in the categories of Rape & Attempted and non-Domestic Aggravated Assaults (214 in 2023 to 249 in 2024).
- In **District A15**, in 2024, there was an increase in Auto Theft (19 in 2023 to 20 in 2024), Other Larceny (86 in 2023 to 125 in 2024) and Commercial Burglary (2 in 2023 to 6 in 2024) for Part One Property Crime categories. With regard to Part One Violent Crime, there were increases in Rape & Attempted, Robbery (12 in 2023 to 18 in 2024) and non -Domestic Aggravated Assault (18 in 2023 to 28 in 2024).
- **Citywide:** Commercial Burglary and Other Larceny increased in Part One property crime for 2024; while only Aggravated Assaults (domestic and non-domestic) increased in Part one violent crime in 2024.

Boston Calls for Service Data 2024:

- In **District A1**, in 2024 DV and SA increased by +95, prostitution by +5 incidents, gang incidents decreased by -23 and overdose incidents increased by +30 from the 2023;
- In **District A15**, in 2024, there where decreases in DV/SA (-39), prostitution (no change), gang involved incidents (-7) and overdose (-21).

• Citywide, in 2024, DV/SA (+735), prostitution (+9) and gang relate crime (+313) increased, while overdose incidents decreased by (-451).

MV Crash Data:

- **District A1,** total crashes in 2024 increased from 1029 to 987 in 2023, 152 with injuries compared to 140 in 2023, 1 fatal compared to 0 fatal in 2023, 84 pedestrian-involved compared to 80 in 2023 and 37 bicycle-involved verse 47 in 2023.
- **District A15**, total crashes decreased in 2024 with 262 compared to 371 in 2023; 50 with injuries compared to 46 in 2023; 1 fatal in 2024 and 1 fatal in 2023; 14 pedestrian-involved compared to 12 in 2023 and 17 bicycle-involved verse 11 in 2023.

Caveats: The 3 categories that we report here, i.e. calls for service, incident report and motor vehicle collision data, only include the 2 Districts that directly abut Encore Casino property. When reporting these abutting District data sets, we cannot assert a direct cause and effect to casino operations. Nor could we if we compared these District data sets to that of other COB Districts, that don't abut the properties. However, by analyzing this data, **we can assume correlation**, based on general increases in patterns of calls for service, incident reports and motor vehicle categories.

With regard to money laundering, sex trafficking, sexual assault, drug facilitated sexual assault, and gang activity related to casino sex trafficking, guns and drug sales; the crime increases in these categories are not significant within crime incident reporting data. Instead, the BPD has worked with other law enforcement agencies assigned to the casino on these crime categories, and over the past four years, there is evidence that these investigative activities are taking place.

Furthermore, as we noted in past applications, there are some crime categories that are difficult to quantify, as they are often not reported, thus do not show up via calls for service data, crime incident data or arrest data. Crimes most notable for underreporting include domestic violence, crimes against children, sexual assault, prostitution, gang-related crime activity (not a crime incident category, but instead a call for service data point) and overdose (also a call for service data point).

Please describe the project in detail and how the proposed project will address the impact indicated above. Please include a breakdown of the proposed scope of work, the scope should be sufficiently detailed to allow the review team to understand the steps required for project completion.

Scope of Work:

The BPD's Public Safety response will include strategies from 7 separate BPD internal Bureaus/Units/Offices. These internal sworn and civilian entities include:

- 1) The Office of the Police Commissioner (OPC)/ Office of Research and Development (ORD),
- 2) The Bureau of Investigative Services' (BIS) Family Justice Center's (FJC) Human Trafficking Unit (HTU),
- 3) BIS Drug Control Unit (DCU),
- 4) BIS Fugitive Apprehension Unit (FAU),
- 5) The Bureau of Field Services' (BFS) Youth Violence Strike Force (YVSF),

- 6) The BFS Area A1 & A15, and a new BPD Unit,
- 7) The BFS's Commercial Vehicle Enforcement Unit (CVEU)

1) The Office of the Police Commissioner (OPC) / Office of Research and Development (ORD)

The ORD is organizationally situated within the OPC, and is responsible for the research and development of innovative strategies and police practices that focus on issues relevant to modern policing. With that mandate, the ORD is responsible for the resource development of an estimated 13 million dollars per year; the administration, management and implementation of an estimated 28 million per year; and oversight of all research related projects that come to the BPD from internal and external partners.

The ORD is currently administering the implementation of (3) MGC CMF grant programs. Once the FY 2026 MGC CMF Block grant is funded; ORD will be administering the implementation of (4) MGC grant programs. This responsibility includes program implementation, financial management and reporting tasks. In addition, the ORD is interested in participating fully in all future MGC research projects, as we believe that we have a great deal to offer these research conversations, including; BPD crime data, organized and cleaned for quality use by MGC researchers, so that the MGC may gain the greatest benefit from their research projects. Finally, ORD would like to help address one of the challenges that has come to ORD's attention while convening planning meetings for this application.

During the January 5th 2024 planning meeting, various BPD units met to discuss the grant application, and the needs presented as a result of Encore Casino operations. Commanders of these Units communicated the need for the BPD to coordinate and communicate better with all other law enforcement agencies working in, and around Encore Casino properties. Several Units communicated at the planning meeting, that they have tried to reach out to other law enforcement agencies involved in casino operations, including the MSP and casino security staff, with little to no communication back. Other Units had described a good relationship with those partners (i.e. the Human Trafficking Unit and Area A). During this meeting, Unit Commanders suggested that we form a partnership with all other law enforcement agencies; especially given the varying types of crime activity that appear to be taking place as a result of casino operations, (i.e. money laundering, sex trafficking, drug dealing, domestic violence, sexual assault, gun related crime, etc.). Given this expressed need, as this partnership grows, the ORD will need support from the MGC to continue and expand this work, including:

- 1) Administering, implementing and financially managing the many strategies proposed in our current and future MGC grant programs;
- Staffing of both internal and external law enforcement partnerships, collaborations and communication efforts - ongoing through the development and staffing of an *Encore Casino Public Safety Task Force (Task Force)*;
- 3) Through that Task Force, facilitate the development of Memorandums of Understanding (MOU), amongst and between law enforcement agencies involved in casino operations; and
- 4) Provide to the MGC, the crime and research data needed for the MGC to effectively analyze the impact of gaming on Boston crime incidents in all Boston neighborhoods, while participating in any and all research partnerships embarked upon by the MGC.

Mitigation Strategy: In the 2025 Public Safety proposal, the MGC funded this position. Recently the BPD's Human Resources Department notified that the position will be posted within the month of February, and likely be hired by July 1, 2025.

The full time *Public Safety Program Coordinator* efforts will help to staff the MGC's future public-safety collaborative efforts, with all other law enforcement partners that serve the Encore Casino community. The entity by which this would happen would be an *Encore Casino Public Safety Task Force* – staffed by the *Public Safety Program Coordinator*. *Please see the attached DRAFT job description. This strategy would represent a mitigation strategy **not noted** in the RFP, but which may be effective in preventing and/or reducing crime incidents in and around the casino properties. In addition, this position will help with a mitigation strategy **noted** in the RFP- *Efforts to better track casino related crimes* - as the Program Coordinator will provide the crime data and analysis needed to track casino related crime.

Budget:

The BPD <u>will not need any funding for this position from July 1, 2025 through June 30th, 2026</u> as we currently have 2025 funding to cover that position from July 1, 2025 through June 30, 2026 at an MM6 Step 1 grade. However, given this 2026 grant will cover a 3-year time frame, we would like to utilize this year's grant to cover the two remaining years of July 1, 2026 – June 30, 2027 and July 1, 2027 through June 30, 2028. This cost will include 2 years of salary (MM6 Step 2 & 3) and fringe (25.45%), and equal <u>\$175,33175</u>. Please see the attached excel spreadsheet for details on these costs.

ORD Subtotal: \$175,33175

2) The FJC's Human Trafficking Unit (HTU):

The BPD HTU is charged with three separate and distinct activities: 1) investigating criminal enterprises that profit from the sexual exploitation of vulnerable persons; 2) identifying and arresting buyers who drive the sex trade by increasing demand for the purchase of vulnerable persons; and 3) following up on referrals that come into the HTU by family members and service providers, of young persons that are missing, and assumed to be coerced, exploited and forced into Boston's sex trade. Follow-up activities are time-intensive and investigative in nature.

The sex trade industry in Boston, nationally and internationally is a multi-billion-dollar industry that has the financial means to constantly evolve and evade criminal prosecution through the use of new technologies, social media, normalization of sexual exploitation, online sale sites, transportation systems, untraceable monetary tools, and force and intimidation of victims. Human trafficking is the fastest-growing and second-largest criminal industry in the world, generating roughly \$150.2 billion worldwide. According to the *Trafficking in Persons Report* produced by the US State Department, approximately 27 million women, children and men are victims of some form of human trafficking; while 79% of human trafficking is sex trafficking.

Evidence that the operation of the Encore gaming facility has caused or is causing this impact is as follows:

The Human Trafficking Unit (HTU) has been working several investigations directly involved with Encore Casio for the past few years. These investigations are open & ongoing.

- The HTU is engaged with in an investigation with the Massachusetts State Police High-Risk Victims Unit, the Massachusetts State Police Gaming Enforcement Unit, the Encore Anti-Laundering Unit, and Encore Security related to individuals believed to be involved in a variety of crimes based out of Roxbury and Dorchester. These individuals are believed to **laundering their illicit gained funds** through Encore where they frequent on a near daily basis.
- The HTU continues to move toward trial of an individual charged with **sex trafficking women**. The victims described being on the Encore Casino floor looking for dates on multiple dates before our

alleged sex trafficker was arrested by the HTU in Boston. The detectives are currently traveling between Boston and Portland, Oregon prepping for this trial.

• The HTU is also assisting the Massachusetts State Police High-Risk Victims on an investigation of an individual who the Boston Police HTU has under active indictment for Human Trafficking and is suspected of **selling designer club drugs** at Encore and at bars in the Seaport area of Boston, in the furtherance of his Human Trafficking operation.

New cases: The Massachusetts State Police Gaming Unit and the Human Trafficking Unit continue to work on various investigations across Boston's neighborhoods with a nexus to the Encore Casino. We continue to target demand in the Charlestown area by focusing on Encore patrons who purchase sex on websites. In October & November 2024, the Human Trafficking Unit along with our partners at the Massachusetts State Police High-Risk Victims Unit and the FBI Child Exploitation & Human Trafficking Task Force conducted operations. During the operation, 11 individuals were charged. Two of these individuals were charged with attempted rape of a child, enticement of a child, and solicitation of prostitution. These investigations are currently also open and ongoing. Just last week, the Human Trafficking Unit charged 2 individuals with Human Trafficking, both of these individuals were operating out of Encore and in a Boston neighborhood. We expect to charge several other individuals in the coming weeks related to these investigations.

The HTU continues and will continue to conduct demand reduction operations targeting sex-buyers that frequent the Encore Casino.

The HTU has also opened a Human Trafficking Drop-In Center in close proximity to the highway where investigators connect victims of human trafficking to basic need supplies and services. At the human trafficking drop-in center victims have the opportunity to shower, receive new clothes, and be connected with services to assist with their future.

Finally, as with all other 8 BPD Bureaus/ Units / Officers; the HTU will be required to attend all of the Encore Casino Public Safety Task Force Meetings. Given that, within this FY 2026 application, the HTU would like to request the following support items, through the City of Boston's (COB) block grant:

Overtime for HTU Detectives to work during hours that are not within their weekly tour of duty work hours, on joint law enforcement surveillance, demand reduction and other activities, to investigate and mitigate criminal activity on casino property, or related to casino activity in the crime categories of sex trafficking, money laundering and related drug trafficking. Estimated cost: **\$30,000** (\$30,000 / \$71.71 per hour = 418 hours)

Training for HTU Detectives: the BPD needs training in a wide range of areas to keep up with the evolving nature of the sex trade, and to understand the tools that they could use for harm reduction. Through this grant, the BPD's HTU will go to The <u>Juvenile Sex Trafficking (JuST) Conference</u> on October 22nd, 23rd and 24th 2025 in Phoenix, Arizona. The Juvenile Sex Trafficking (JuST) Conference was born in 2013 out of a vision to bring high-quality training to a national audience while simultaneously fostering collaboration at all levels of the community. This nationally-renowned conference features today's most pressing issues facing professionals and advocates in the anti-trafficking field. Workshops focus on skill-building, survivor experiences, cross-discipline collaboration, case studies, and lessons learned in the areas of investigation, prosecution, and therapeutic services. Each year offers new speakers and an array of powerful new workshops, with the goal of inspiring action and educating attendees on how to tackle child and youth sex trafficking from all sides. The BPD's HTU would like to send 5 HTU Detectives this year at a cost of **\$10,000**, which will include conference

registration, round trip airfare and 2-night stay at a hotel in close proximity to the conference, ground transportation and per diem food expenses.

2 Dell XPS 17 Laptops are needed for HTU Detectives to utilize within their vehicles while conducting surveillance activities at the ENCORE, to do background checks and other investigative tasks. These 2 laptops will cost as estimated **\$4,400.00**

2024 Honda Odyssey: The HTU would like to purchase a 2024 Honda Odyssey to 1) transport victims of casino sex trafficking cases to safe shelter locations statewide, so that they may feel safe while being interviewed and supported through each phase of investigation, prosecution and to start a new journey moving forward; and 2) to conduct surveillance while at Encore Casino properties. This vehicle will blend in with other casino vehicles, and not cause attention by presenting as a law enforcement vehicle. Estimated cost: **\$42,987.00**

HTU Subtotal: \$87, 387.00

3) BIS Drug Control Unit (DCU):

The DCU is dedicated to the enforcement of the controlled substance laws as they pertain to manufacturing and distribution. Our officers have established proven, legally sound methods of investigation to combat drug-related crimes from street level up to major trafficking levels to eradicate not only the dealers themselves, but also the various associated quality of life crimes such issues can present. The officers of the DCU are dedicated to combatting drug-related issues in every neighborhood in the COB, as well as working in collaboration with our neighboring law enforcement agencies to improve the quality of life for all residents.

The DCU officers are vastly experienced in investigating and preparing for the prosecution of criminals and drug gangs who perpetrate violence, fear and intimidation in our communities. These investigations often lead to not only the arrest and prosecution of those responsible for drug manufacturing and trafficking, but also the seizure of illicit drugs, illegal firearms and forfeiture of assets derived from drug traffickers at various levels in Boston and surrounding cities and towns. Boston DCU officers are extensively trained and highly skilled in undercover work, gathering surveillance, conducting drug buys, recruiting confidential informants as well as court preparation and testimony, and maintaining evidential and operational logs for long-range investigations. The DCU has coordinated and cooperated with state and local law enforcement officials on mutual drug enforcement efforts. With that said, the 70 officers assigned to DCU are at your disposal and available to work with the Encore Security team, Everett PD and Mass State Police in any capacity needed in a joint-efforts to combat drug activity at the Encore Casino.

The DCU has engaged many times in operations, jointly, with federal partners that have occurred, in part, at the Encore Casino. The officers assigned to the DCU, have acted in an undercover capacity, for the purpose of furthering investigations for crimes such as drug trafficking and money laundering. Some of these investigations are ongoing and giving details of these and other operations may jeopardize the integrity of the investigations and the officers. Through our participation in these investigations, we are aware that drug related crimes, and others, take place within the Encore Casino and its surrounding properties. Since 2021, money seized as proceeds from drug related crimes, has consistently decreased. According to the Department's Financial Evidence Office, in 2021, money seized by Boston officers totaled \$1,446,781.91; In 2022, it was \$948,129.75 and in 2023 the total was \$500,498.44. It can be

reasonably inferred that there is a correlation of dwindling totals of seized drug money proceeds since the opening of the Encore Casino in 2019.

Although the BPD's DCU does not yet have a formal relationship with law enforcement LE agencies working in and around casino properties, they are very interested in meeting with those LE agencies and establishing a Memorandum of Understanding (MOU) with them to participate in multi-agency drug-related surveillance and crime investigation activities. The DCU, after establishment of an MOU, will utilize **Overtime (OT)** to participate in joint operations over the life of the grant. They will keep track of OT use, surveillance efforts, investigations, arrests and other related activities. In addition, officers of the DCU will actively participate in the *Encore Casino Law Enforcement Task Force* meetings and follow up strategies and investigations that come forth through increased collaboration and communication.

Impact: At this point in time, as the DCU has not yet operated in partnership with other LE partners on these activities, we assume that the impact will show in a decrease in drug-related crime incidents (i.e., overdose, drug sales, DFSA, illegal gun possession, etc.) on Encore properties, and the prevention of these crimes as it becomes apparent to drug dealers that LE is present and surveilling their activities.

Budget:

Overtime - \$6,000 / \$71.71 per hour = 84 hours

DCU Subtotal: \$6,000

4) BIS Fugitive Apprehension Unit (FAU);

The BDP FAU's primary function is to locate and apprehend individuals wanted for serious crimes. Requests for assistance in this matter come from a wide variety of sources both inside the BPD, and from partners both locally and federally. Fugitive Investigations include undercover surveillance both on foot and in unmarked vehicles. These activities are time intensive and investigative in nature. The Boston FAU currently works closely with the Massachusetts State Police Violent Fugitive Apprehension Section (VFAS), who works in and around the Encore Casino. The Boston FAU assists in surveillance and arrest of individuals wanted by the Massachusetts State Police (VFAS), and anticipates that we will assist in fugitive investigations in and around the Encore Casino in the future; as in the past, the Boston FAU has contacted the head of security for the Encore Casino, and the Massachusetts State Police Gaming Unit in furtherance of a fugitive investigations. The Boston FAU anticipates future Fugitive Unit investigations will include surveillance and apprehension in and around the Encore Casino of high-risk offenders.

Identified Impacts:

- **Increased visitation** and employment due to the casino will likely increase the interaction between public safety personnel and **casino patrons** and employees.
- It is recognized by law enforcement and the casino industry that certain types of crime may be attracted to casinos and other hospitality related businesses. These may include human trafficking, money laundering, and drug trafficking. Other crimes that may be attributable to casinos include increased assaults, fraud, and property crimes.

Mitigation Measures:

- **Collaborative** efforts to identify, monitor and address issues related to human trafficking, drug trafficking and money laundering.
- Collaborative efforts to better track casino related crimes

Strategy: The BPD FAU will participate in the *Encore Casino Public Safety Task Force* collaborative meetings and efforts, with both internal BPD Units and external law enforcement partners, to communicate more consistently about casino related and/or originated crime. And through that increased collaboration and communication, the FAU will be more involved in future joint law enforcement apprehension efforts - as these operations will likely focus on known high-priority repeat offenders, who frequent casinos to commit crimes, **many of which may be fugitives of the law**.

Overtime for FAU Personnel: The FAU will need an estimated **\$6,000.00** in overtime to participate in internal and external planning meetings, surveillance and follow casino related investigations. 6,000 /**\$71.71 per hour = 84 hours**

Subtotal for FAU: \$6,000.00

5) The Bureau of Field Services' (BFS) Youth Violence Strike Force (YVSF);

The YVSF's goal is to reduce the criminal activity and anti-social behavior of youthful offenders and youth gangs through directed and community-based policing strategies. The YVSF is very interested in meeting with those LE agencies that work with Encore and establishing a Memorandum of Understanding (MOU) with them to participate in multi-agency surveillance and crime investigation activities related to Boston-based gang affiliates. Anecdotally, it is believed that gang-related criminal activity, including drug sales, prostitution, and money laundering is taking place inside and around casino property. The YVSF, after establishment of an MOU, will utilize OT (\$5,000) to participate in joint operations over the life of the grant. We will keep track of OT use, surveillance efforts, investigations, arrests and other related activities.

The Youth Violence Strike Force which is commonly known as the Boston Police Gang Unit, is a multi-faceted investigative and proactive unit, that is able to pull illegal firearms off the streets and stop violent acts from happening throughout the city and surrounding areas. At the same time, we are able to interact with the community through various inroads that we have made with the many stake holding groups in and around Boston.

Since 2021 this unit has arrested approximately 900 impact players driving the violence in and around the city. During that same time, we were able to pull 359 illegal firearms off the streets, a majority of which were carried by known gang members who were carrying these weapons to inflict harm. The various other arrests the Youth Violence Strike Force make are not for low level non-prosecutable misdemeanors, but rather high-level felonies where there is a victim on the other end who has been put in fear. We are also tasked by various agencies throughout the Commonwealth and beyond to assist them in a variety of ways including but not limited to Homicide investigation and apprehensions. We also work closely with the Mass State Police, US Marshalls, ATF and most of the surrounding municipalities throughout New England. We have also learned that there are various Boston Gangs laundering their illegal proceeds at Encore Casino as a way of cleaning their money.

Understanding the complexities of Policing in 2024 has enabled this unit to evolve into what it is today. We have established an unlimited amount of contacts within the community. Our purposeful engagement with each and every neighborhood in the city has allowed us to reach certain youth before they may be influenced by the bad actors who could steer them down the wrong path. Our partnerships include Ella J. Baker House and Youth Violence Reduction Task Force, Operation Exit, ROCA and Youth Connect, just to name a few. We realize that in order to build trust within the community we had to do more than make arrests. These partnerships have allowed us to be a partner with the community that we police, not a force of opposition.

Overtime for YVFS Personnel: The FAU will need an estimated **\$6,000.00** in overtime to participate in internal and external planning meetings, surveillance and follow casino related investigations. \$6,000 / \$71.71 per hour = 84 hours

Subtotal for YVSF: \$6,000.00

6) BFS Area A1 & A15:

Area A1 (Downtown Boston) and A15 (Charlestown) Districts have been negatively impacted by increased traffic flow in and around the roadways connected to the Encore Casino, including motor vehicle, pedestrian and bicycle collisions. In addition, there has been an increase in car thefts and motor vehicle larcenies. To combat the continuance of these incidents, the BPD needs high-visibility patrols in the area between 4:00 p.m. through 4:00 a.m., during high-traffic weekend days and evenings (Friday and Saturday). In addition, Detectives from these two Districts need additional time to implement strategies to combat the increase in car theft and larceny. These strategies could include undercover surveillance, community meetings to educate residents about target hardening tactics that protect their cars from theft and larceny, and additional hours to investigate car theft and larceny incidents. Overtime funds will be needed for all of these activities.

For this year's 2026 application, Area A, would also like to purchase **4 motorcycles and the uniforms and equipment** that are needed for a motorcycle patrol. The need for motorcycles to patrol the Encore properties and surrounding Area A jurisdiction has become more apparent as time has passed. With increased traffic congestion, and new construction projects coming on line each year, the BPD has realized the need for a quicker and more flexible response to traffic safety incidents, violations, collisions, heavy traffic, and traffic in poor weather; response that can only be served by the speed and flexibility of a motorcycle patrol. Motorcycles are best suited for these roles because of their quick deployment and ability to conduct traffic enforcement in urban areas. Additionally, Motorcycles can be used to close streets and other traffic diversion functions or interest related to high profile guests. Motorcycles have a long and proven history as the best option for traffic enforcement in urban centers.

Identified Impacts that will be addressed:

- The presence of casinos has been demonstrated to cause an increase in cases of operating under the influence.
- Increases in traffic can cause increases in congestion, accidents, and vehicular/bicycle/pedestrian conflicts.

Mitigation Strategies that will be employed:

- Efforts to improve traffic safety that could include enhanced traffic enforcement, use of speed/message boards, public education programs, or other efforts that are demonstrated to improve traffic safety.
- Efforts to reduce impaired driving potentially including sobriety checkpoints, saturation patrols, education programs, or other demonstrated measures to reduce impaired driving.

Budget:

- Overtime for 2 dedicated traffic officers, 12 hours per day (4:00 pm 4:00 am), 2 days (F, S) a week (2 X 2 days X 12 hours X \$71.71 per hour = \$3,443.08 per week X 52 weeks per years = \$178,988.16 / \$71.71 per hour = 2,496 hours
- 2024 Harley -Davidson Motorcycles / FLHP Road King 4 X \$9,875.00 = \$119,956.00
- Gore-Tex Hi-Vis Super-shell Jacket $-4 \times $519.99 = $2,079.96$
- Dark Navy Fleece Jackets \$110.96 X 4 = \$443.84

- Shipping \$12.00
- Frisk-master All-leather, Cut Resistant, Police Duty Glove 4 X \$48.95 = \$195.80
- Neos Adventure 15" Overshoes \$107.95 X 4 = \$431.80
- Shipping \$36.00
- Police Field Boots 4 X \$625.00 = \$2,500.00

Area A Subtotal: \$304,643.56

*As with all other BPD Bureaus/ Units / Officers; Area A supervisors will be required to attend all of the *Encore Casino Public Safety Task Force Meetings*.

8) The BFS's BSO Commercial Vehicle Enforcement Unit (CVEU) -

The Boston Police Department's Commercial Vehicle Enforcement Unit (Unit) is under the Bureau of Field Services and part of the Bureau of Special Operations. The Unit is composed of 3 officers and a Sergeant. Other than the Mass State Police Truck Team, the Unit is one of only 3 full time local law enforcement units solely dedicated to enforcing Federal Commercial Motor Vehicle Law in the State of Massachusetts. All officers assigned to the Unit have undergone approximately 3 years of specialized training and are Certified USDOT FMCSA Safety Inspectors. Through their certifications all members are authorized to conduct warrantless inspections on all property and passenger carrying commercial motor vehicles (CMV) on a public way. As per MGL Chapter 90, which deals with motor vehicle law, in order for a police officer to conduct a motor vehicle stop a violation must be observed. However, due to the extensive federal training and vetting process, the officers assigned to the Unit can conduct motor vehicle stops on these CMV's without observing a traffic violation. They are the only officers in the Boston Police Department that are authorized to do so. Additionally, the Unit is the only specialized group within BPD that are authorized to enforce Federal Motor Carrier Law, levy fines and confiscate those vehicles found to be unsafe or unfit for road travel.

Human traffickers often use commercial vehicles, including trucks, planes, trains and ships, to transport victims. [Human Trafficking Response Guide for the Transportation Industry, U.S. Department of Transportation] In fact, human trafficking can heavily depend on transportation systems in order to operate. For example, traffickers often use transportation hubs to recruit victims of sex trafficking and forced labor. Once recruited, traffickers may utilize transportation systems in order to relocate victims to other places where they will be abused and exploited.

The BPD's CVEU is in a unique position to recognize and report potential instances of human trafficking in the course of their work with commercial vehicles in the City of Boston. Unfortunately, none of the officers have been formally trained on how to detect signs of human trafficking. Furthermore, the unit lacks critical technology that would enable them to detect humans in the cargo holds of commercial vehicles. There are currently no efforts being made in this area. Therefore, the goal of this project is to expand the work of the BPD's Commercial Vehicle Enforcement Unit (CVEU) into a new area to better serve the Encore community, in and around the Encore properties.

Budget Request:

Training: Truckers Against Trafficking Training- <u>https://train.tatnonprofit.org/</u> September 21st through September 25th, 2025, Denver, CO – 3 BPD participants:

- Registration: 750.00 X 3 = 2,250.00
- Hotel Accommodations: 3 participants X 5 nights days X \$350.00 per night = \$5,250.00
- RT Flights: 3 participants X 350.00 = 1,050.00

\$8,550.00

Overtime for additional truck stops and inspections, OT and OT for backfill for participating in trainings. Overtime: \$82.00 x 7.5 hrs. = \$615 PER SHIFT, \$615 x 3 members= \$1,845 A WEEK, \$1,845 X 52 weeks = \$95,940

CVEU Subtotal: \$104,490.00

Public Safety Strategy Total: \$758,837.54

City of Boston's / Boston Police Department's Public Safety Strategy Response to the 2026 Mass Gaming Commission's Block Grant Program Proposal Attachments

- Crime Data & MV Crash Data
- Calls for Service Data
- Budget Worksheet
- Law Enforcement Coordinator Job Description MM6 Step 1
- Memo from Area A Captain Driscoll justifying use of Motorcycles around Encore jurisdiction
- Quote for the Motorcycles
- ✤ Quotes for Motorcycle Uniforms and Supplies
- Quote for the HTU's Honda Odyssey
- Dell XPS17 Specifications the BPD's ISG Unit has a contract for which they are \$2,200 each
- ✤ JUST Training Conference October 14th 16th, 2025, Orlando, FL
- Truckers Against Trafficking Training Website
- BPD's Public Safety Strategy Timeline



Maria Cheevers <maria.cheevers@pd.boston.gov>

FY 2026 MGC Grant - crime incident data and traffic collision data

Elizabeth Curtis <elizabeth.campbell@pd.boston.gov> Fri, Jan 17, 2025 at 11:44 AM To: Maria Cheevers <maria.cheevers@pd.boston.gov>, John Patrick Moylan <johnpatrick.moylan@pd.boston.gov>

Crime Data

- In **District A1**, from 2023 to 2024, the only Part One property crime category that saw an increase was Other Larceny (1,342 to 1,402); while Part One violent crime was only up in the categories of Rape & Attempted and non-Domestic Aggravated Assaults (214 in 2023 to 249 in 2024).
- In **District A15**, from 2023 to 2024, there was an increase in Auto Theft (19 in 2023 to 20 in 2024), Other Larceny (86 in 2023 to 125 in 2024) and Commercial Burglary (2 in 2023 to 6 in 2024) for Part One Property Crime categories. With regard to Part One Violent Crime, there were increases Rape & Attempted, Robbery (12 in 2023 to 18 in 2024) and non Domestic Aggravated Assault (18 in 2023 to 28 in 2024).
- **Citywide:** Commercial Burglary and Other Larceny increased in Part One property crime for 2024; while only Aggravated Assaults (domestic and non-domestic) increased in Part One violent crime in 2024.

MV Crash Data:

- **District A1,** total crashes in 2024: 1029 (987 in 2023), 152 with injuries (140 in 2023), 1 fatal (0 fatal in 2023), 84 pedestrian-involved (80 in 2023), 37 bicycle-involved (47 in 2023).
- District A15, total crashes in 2024: 262 (371 in 2023), 50 with injuries (46 in 2023), 1 fatal (1 fatal in 2023), 14 pedestrian-involved (12 in 2023), 17 bicycle-involved (11 in 2023).

If you need anything else, just let me know.

Have a nice weekend, thanks!

Liz Curtis Deputy Director Boston Regional Intelligence Center Boston Police Department Elizabeth.Campbell@pd.boston.gov 617-343-6520

[Quoted text hidden]

of Calls in the District and Citywide

			2018		
	DV/SA*	Prostitution	Gang	Overdose	ALL CFS
A-1	069	5	401	246	84,299
A-15	319	1	60	23	14,166
Citywide	19,268	173	2,597	1,417	673,419

DV/SA Prostitution Gang 582 6 352 357 0 26 10 770 11.5 270				5102		
1 582 6 5 357 0 15 10 10 115 115 115 115		DV/SA	Prostitution	Gang	Overdose	ALL CFS
357 0 0	A-1	582	9	352	300	85,876
115 1077 01	A-15	357	0	26	14	14,450
011 6//01	Citywide	18,779	116	2,378	1,650	686,895

			2020			
	DV/SA	Prostitution	Gang	Overdose	ALL CFS	
A-1	543	2	320	245	69,082	A-1
A-15	260	0	38	24	12,121	A-15
Citywide	18,577	123	2,999	1,473	642,185	Cityw

			2021			
	DV/SA	Prostitution	Gang	Overdose	ALL CFS	
A-1	560	7	263	235	69,706	A-1
A-15	228	0	34	20	11,609	A-15
Citywide	18,201	126	1,960	1,443	612,306	City

			2022		
	DV/SA	Prostitution	Gang	Overdose	Total
A-1	564	3	251	281	60,707
A-15	264	1	26	18	9,922
Citywide	18,568	54	1,687	1,843	600,753

			2023		
	DV/SA	Prostitution	Gang	Overdose	ALL CFS
A-1	532	0	296	265	65,204
A-15	268	0	40	29	9,949
Citywide	18,422	38	2,256	1,992	660,943

			2024		
	DV/SA	Prostitution	Gang	Overdose	Total
A-1	627	5	273	295	62,790
A-15	229	0	33	ø	9,496
Citywide	19,157	LÞ	2,569	1,541	645,107

* "dvip" Or "nidv" Or "cd24" Or "viordr" Or Like "*sexast*" Or Like "*stalk*" Or Like "*tape*"

% of Calls in the District and Citywide

			OTU2		
NG	DV/SA	Prostitution	Gang	Overdose	ALL CFS
A-1	1%	%0	%0	%0	
A-15	2%	%0	%0	%0	100%
Citywide	3%	%0	%0	%0	100%

			2019		
-	DV/SA	Prostitution	Gang	Overdose	ALL CFS
A-1	1%	%0	%0	%0	100%
A-15	2%	%0	%0	%0	100%
Citywide	3%	%0	%0	%0	100%

			2020		
	DV/SA	Prostitution	Gang	Overdose	ALL CFS
A-1	1%	%0	%0	%0	100%
A-15	2%	%0	%0	%0	100%
Citywide	3%	%0	%0	%0	100%

				2021		
		DV/SA	Prostitution	Gang	Overdose	ALL CFS
A-1	1	1%	%0	%0	%0	%001
¥	۱-15 v-15	2%	%0	%0	%0	
Ð	Citywide	3%	%0	%0	%0	100%

Prostitution	Gang	Overdose	Total
%0	%0	%0	100%
%0	%0	%0	100%
%0	%0	%0	100%
	Prostitution 1% 0% 3% 0% 3% 0%	Prostitution Gang (0% (0% (0% (0% (0% (0% (0% (0%	Prostitution Gang Overacise 0% 0% 0% 0% 0% 0%

			2023		
	vs/na	Prostitution	Gang	Overdose	ALL CFS
A-1	%1	%0	%0	%0	100%
A-15	%E	%0	%0	%0	100%
Citywide	%E	%0	%0	%0	

L

			2024		
	DV/SA	Prostitution	Gang	Overdose	Total
A-1	1%	%0	%0	%0	100%
A-15	2%	%0	%0	%0	
Citywide	%E	%0	%0	%0	100%

2026 Boston Police De		s Gaming Co nent's Public			ron	معا		-
				•	-			
	7/1/2	5-6/30/26	7/1/2	26-6/30/27	7/1/:	27-6/30/28		Total
Personnel								
MCG LE Program Coodinator	<u> </u>				ļ		ļ	
Salary	\$	-	\$	68,848.40	\$	70,913.85	\$	139,762.25
Fringe 25.45%	<u>\$</u>	<u>.</u>	<u>\$</u>	17,521.92	\$	18,047.58	\$	<u> </u>
Subtotal	\$	=	\$	86,370.32	\$	88,961.43	\$	175,331.75
Overtime				······································				
BIS Human Trafficking Unit (HTU)	\$	10,000.00	\$	10,000.00	\$	10,000.00	\$	30,000.00
BIS Drug Control Unit (DCU)	\$	2,000.00	\$	2,000.00	\$	2,000.00	\$	6,000.00
BIS Fugitive Apprehension Unit (FAU)	\$	2,000.00	\$	2,000.00	\$	2,000.00	\$	6,000.00
BFS Youth Violence Strike Force (YVSF)	\$	2,000.00	\$	2,000.00	\$	2,000.00	\$	6,000.00
BFS Area A1 &A15 - Overtime for 2 dedicated traffic officers, 12 hours per day (4:00 pm - 4:00 am), 2 days (F, S) a week (2 X 2 days X 12 hours X \$71.71 per hour = \$3,443.08 per week X 52 weeks per year = \$178,988.16 / \$71.71 per hour =								
2,496 hours	\$	59,662.72	\$	59,662.72	\$	59,662.72	\$	178,988.16
BFS Special Operations' Commercial Vehicle						, ,	<u> </u>	
Enforcement Unit (CVEU)	\$	31,980.00	\$	31,980.00	\$	31,980.00	\$	95,940.00
Subtotal	\$	107,642.72	\$	107,642.72	\$	107,642.72	\$	322,928.16
								,
Training								
Human Traffikcing Unit US DOJ Conference 5								
participants	\$	10,000.00	\$	_	\$	-	\$	10,000.00
BFS Special Operations' Commercial Vehicle	¥		 ₩		<u> </u>		 [↓]	10,000.00
Enforcement Unit (CVEU) Truckers Againgst								
Trafficking Training - 3 BPD officers	\$	8,550.00	\$		\$		\$	8,550.00
Trantieking Training "5 DI D officers	\$	18,550.00	\$	-	\$		\$	18,550.00
	φ	18,550.00	\$	-	ф.	-	3	18,550.00
Equipment								
	\$	4 400 00	\$		¢		6	4,400.00
HTU Dell XPS 17 Laptop 2 X 2,200	\$	4,400.00	<u>}</u>	-	\$		\$	4,400.00
Area AGore-Tex Hi-Vis Super-shell Jacket – 4 X								
\$519.99 = \$2,079.96	\$	2,079.96	\$	-	\$		\$	2,079.96
Dark Navy Fleece Jackets - \$110.96 X 4 =								
\$443.84	\$	443.84	\$	-	\$	-	\$	443.84
Shipping - \$12.00	\$	12.00	\$	-	\$	-	\$	12.00
Frisk-master All-leather, Cut Resistant, Police								
Duty Glove – 4 X \$48.95 = \$195.80	\$	195.80	\$	-	\$	-	\$	195.80
Neos Adventure 15" Overshoes \$107.95 X 4 =								
\$431.80	\$	431.80	\$	-	\$	=	\$	431.80
Shipping \$36.00	\$	36.00		-	\$		\$	36.00
Police Field Boots 4 X \$625.00 = \$2,500.00	\$	2,500.00			\$	-	\$	2,500.00
	\$	10,099.40	\$		\$		\$	10,099.40
	Ψ	10,077.40	\$	-	. Ф		\$	10,099,40
Vechicle								·· · · · · · · · · · · · · · · · · · ·
HTU 2024 Honda Odyssey	\$	42,987.00	\$	_	\$	-	\$	42,987.00
Harley-Davidson Police Motorcycle 4 X \$9,875.00							*	
	<u>3</u>	119,956.00	<u>\$</u>	<u> </u>	\$	-	<u>\$</u>	119,956.00
	\$	162,943.00	\$	-	\$	-	\$	162,943.00
Subtotal		299,235.12		194,013.04		196,604.15		689,852.31
10% City of Boston Admin.								68,985.23
TOTAL		······································						758,837.54

Boston Police Department (BPD) Program Coordinator / Research Analyst

Funded by the FY 2025 Mass Gaming Commission (MGC) Community Mitigation Fund (CMF)

This is a provisional appointment.

Brief Job Description (essential functions of the job):

The Office of Research and Development (ORD) is organizationally situated within the Office of the Police Commissioner (OPC), and is responsible for the research and development of innovative strategies and police practices that focus on issues relevant to modern policing. The ORD supports and advances the Boston Police Department's mission: *to work in partnership with the community to fight crime, reduce fear, and improve quality of life in the neighborhoods of Boston*.

Under the supervision of the Director of the ORD, the Program Coordinator / Research Analyst will coordinate, manage and administer all MGC CMF grant programs (FY 2022, 2023 & 2025), while also conducting extensive analyses of crime and intelligence data using analytic technology and other statistical software. This position will be required to report back to the MGC, and related Research Consultants hired by the MGC to analyze the impact of Encore casino operations on crime incidents; and report City of Boston (COB) specific crime data. This position will be funded through a grant from the MGC, CMF COB Block Grant, and support the Boston Police Department's (BPD) public safety strategy to mitigate the negative crime impacts of Encore Casino operations.

Job Description (essential functions of the job):

Program Coordinator responsibilities include:

- Administer, implement and financially manage the many strategies proposed in our current and future MGC grant programs;
- Staff both internal and external law enforcement partnerships, collaborations and communication efforts ongoing through the development and staffing of an *Encore Casino Public Safety Task Force*;
- Through that task force, facilitate the development of **Memorandums of Understanding**, amongst and between law enforcement agencies involved in casino operations; and
- Provide to the MGC, the crime and research data needed for the MGC to effectively analyze the impact of gaming on Boston crime incidents, while participating in any and all research partnerships embarked upon by the MGC.
- Grant application planning, collaboration, writing and submission;
- Award and contract acceptance; grant program and budget set up; program implementation, monitoring, evaluation; and quarterly/semi-annual and final reporting; and
- Supporting all BPD Units that have been awarded MGC CM funding, as they purchase equipment and supplies; plan and attend their approved training; personnel Data Analyst responsibilities include:

Working within an analytic team to collect, review, collate, and analyze data and information; producing and disseminating intelligence reports in support of the COB's community mitigation, public safety

strategy, as it relates to Encore Casino operations; contributing to efforts involving the research of various documents, reports, raw data, and internet-based material, to provide the analysis and summarization of results for inclusion in reports and presentations. The position would:

- Utilize analytic software (to include: word processing and publishing, data discovery, data search, process mapping, databases and spreadsheets, pivot tables, link analysis, social media analysis, and geospatial analysis technologies) to augment and facilitate analysis and intelligence production;
- Communicate and collaborate with colleagues and professional partners via multiple platforms to include secure internet/web-based forums, telephone, email and police radio;
- Demonstrates strong oral and written communications skill;
- Comply with legal, policy, and regulatory requirements; and
- Apply sound professional judgment to ensure the protection of confidential information, as well as privacy, civil rights and civil liberties.

Minimum Entrance Qualifications:

Applicants must be permanent or naturalized U.S. citizens. Applicants must possess a bachelor's degree from an accredited college or university and have a minimum of three (3) years of full-time or equivalent part-time paid professional experience where the major duties involved the analysis of information and data, use technology and data management systems, and production of detailed analytic reports, a high level of accuracy and attentiveness to numerical calculation; strong analytical and problem-solving skills; and strong written and oral communication skills. Ability to exercise good judgement and focus on detail as required by the job. It is strongly preferred that applicants have extensive experience using ESRI Mapping software, Microsoft Word, Excel, and Access. A bachelor's degree in Geography, Computer Science, Criminal Justice or related topic may be substituted for two (2) years of the required experience. Upon hire, employee will be asked to obtain and maintain a SECRET level national security clearance.

CRIMINAL RECORD CHECK REQUIRED PRE-EMPLOYMENT DRUG TEST REQUIRED BOSTON RESIDENCY REQUIRED THIS IS A TEMPORARY GRANT FUNDED POSITION

TERMS:

Union/Salary/Grade: SENA/ MM1-6 Step 1 - \$66,843.10 plus benefits Hours Per Week: 35



One Schroeder Plaza Boston, MA 02120

> **Page1** of **1 Date** 1/28/25 **CC#**N/A

District/Unit: A-1

To: Maria Cheevers, Director, of Office of Research and Development

Through: BFS Command

From: Capt. Richard Driscoll, Commander A-1

Subject: Motorcycle Request for Massachusetts Gaming Commission Grant

Hello,

I am requesting consideration for the acquisition of Four (4) Motorcycles through the Mass Gaming Commission Grant. The acquisition of Motorcycles is central to the traffic enforcement and mitigation plan directly connected to the increased vehicular traffic related to the Encore Casino. The motorcycles will be an effective asset to help with traffic issues impacting the surrounding community in District's A-1 and A-15.

A motorcycle is best suited for this role because of its quick deployment and ability to conduct traffic enforcement in an urban setting. Additionally, Motorcycles can be used to close streets and other traffic diversion functions related to events that may occur at the casino related to a function or interest related to high-profile guests. Motorcycles have a long and proven history to be the best option for traffic enforcement in the urban setting.

Specifically, the request is for the same model that BPD currently uses for its Mobile Operations Unit to conduct focused traffic enforcement. This will enable long term maintenance and repair to continue and for the asset (because of interoperability and mechanic familiarity) and will ensure the asset is used for a long period of time to help the Encore Casino area traffic enforcement plan.

Respectfully Submitted,

Capt. Richard Driscoll ID#54108



ast Harley-Davidson

P.O. Box 1740 – 17 Lafayette Road (US Route 1) North Hampton, New Hampshire 03862 Tel: (603) 964-9959 | Fax: (603) 964-9942 seacoastharley.com | nemotorcyclegroup.com



To: Boston Police Department Attention: Sgt. Peter DeMayo Re: Boston Police Motor Unit – Special Operations Date: 09/27/2024

QUOTE

Option 1: Harley-Davidson Police Motorcycle Purchase

(4) 2024/*2025 H-D FLHP Road King (WHT & IMPACT BLU w/BPD PAINT REQUIREMENTS) *\$20,114.00

Equipment Purchase | Installation

Front:

Whelen Windshield LED light array - clear lenses, front; Whelen PAR 36 New Gen Side Spot-Lamps - clear lenses, LED BLU (pair) front; LED headlight (Hogworkz) clear, front; under headlight ION LED bar/light - clear lense, front; H-D rider heated hand-grips, front; microphone & bracket mount, front; Chrome Package 1 (H-D chrome covers, headlamps, spot lamps, turn signals & axel nuts, front; Whelen TION-V (fork mounted - pair - clear lenses) front; Whelen TION-V (spot mounted - pair - clear lenses), front; H-D cooler chassis fan - under Side:

Engine guard LED's, match existing generation (pair) (possibly would need to swap or upgrade - change TBD); side; Love Jugs Cooling fan, side; H-D/Code 3 siren & amp w/bracket, side; H-D saddlebag guard rails (chrome) Rear:

HAVIS box, empty, refurbished & supplied by BPD; add to HAVIS installation - Whelen/or approved LED light kit new (blue lenses, rear brake light top rail, red with amber signal ends), rear; box 12v power supply, rear; H-D brake light kit, rear; Chrome Package 3 (H-D chrome covers, brake, turn, taillights). All lights to be wired/match existing BPD patterns. Does not include engine guard soft-covers, motorcycle covers, or embroidered seat covers.

TOTAL EQUIPMENT ESTIMATE TOTAL WITH EQUIPMENT:

4× \$9,875.00

11995600

- *Factory Pricing, model and specifications are subject to any Harley-Davidson changes
- Factory Rear running lights set around the registration plate (blue) Milwaukee factory standard
- Contract Information Seacoast H-D holds the MASP contract for H-D Motorcycles #SP18-MTRCYCL-F81
- Dealership Service Leased Motors fall under our Priority Maintenance Plan (1^a Service at 1,000 miles)
- Dealership Service Purchased Motors fall under standard Customer Service (Agencies are responsible)
- Dealership Service (2) keys provided | (1) manual provided when requested
- Dealership Delivery We can deliver the Motor(s) to your agency when requested
- Dealership Delivery Depending on our delivery radius *Delivery charges may apply*
- Dealership Delivery Decals & graphics are handled by agency THIS IS A NON-DECAL DELIVERY

See page two – Lease options



Quote# AAA7740

January 6th 2025

Thank you for the opportunity to provide this quote. Please let us know if you have any questions by replying to this email.

Otis Ha OTIS.F 617-34 Ship To 364 W/	DN POLICE DEPARTMENT SPEC OPS arewood IAREWOOD@pd.boston.gov 3-5651	Terms: Net 30 Quoted by: Riley Edwar Valid until: February 6th Created Date: January	า 2025		
			Unit Price	Qty	Extended
	GORE-TEX Hi-Vis Supershell Jacket 9970V-LG-REG Size: LG REG		519.99	ץ	2079.96
	Total Number of Patches To be Installed (Per Garme Customization - Logo:) Other - Specify in Ord Patch Customization (Specify Below - Additional Cha Both Arms - BPD Color Spec Ops Order Notes:) Silver Reflective Logo: BOST SPECIAL (Straight) OPERATIONS (Straig	er Notes Iges May Apply) Patch ON POLICE (Arched)			
	Fleece Jacket 4650-DA-LG-REG Color: Dark Navy Size: LG REG		110.96	ץ	443.84
	Total Number of Patches To be Installed (Per Garme	m) Noné			
		Shipping	Subtotal (Grourid)	\$	Ar CD
	······································		Total	\$	2535.80
	Reply To Quo	te With Your PO			
• cł • •	If you want to make changes to your quo hanges and we'll get a revised quote sent a Pricing is subject to change at any time. We may add additional taxes in some sta Feel free to contact us at <u>serior language</u>	asap. .tes.			d

AAA Police Supply || 940 Providence Highway, Dedham, Massachusetts 02026



Quote# AAA7739

Thank you for the opportunity to provide this quote. Please let us know if you have any questions by replying to this email.

Otis Har OTIS.H 617-343 Ship To: 364 WA	N POLICE DEPARTMENT SPEC OPS rewood AREWOOD@pd.boston.gov	Terms: Net 30 Quoted by: Riley Edwa Valid until: February 6t Created Date: January	h 2025		
			Unit Price	Qty	Extended
	Friskmaster All-Leather, Cut Resistant FM2000 1011054 Sizen Large	Police Duty Glove	48.95	×4	195.80
	Notes: SIZES TBD				
	Neos Adventurer 15" Overshoes ANN1-XL Szcs XL 11.5 - 13		107.95	×4 =	431.80
	Notes: SIZES TBD				
Ni- 1.		Shippin	Subtotal g (Ground)	\$	\$ 36.00
4			Total	\$	1063.60

Reply To Quote With Your PO

 If you want to make changes to your quote please reply to this email with the requested changes and we'll get a revised quote sent asap.

Pricing is subject to change at any time.

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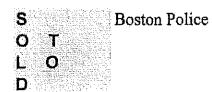
- We may add additional taxes in some states.
- Feel free to contact us at <u>antianantian production of the set of a state of a state of a state of a state</u>.

AAA Police Supply || 940 Providence Highway, Dedham, Massachusetts 02026

ALDEN SHOE COMPANY

1 Taunton Street Middleborough, MA 02346 INVOICE

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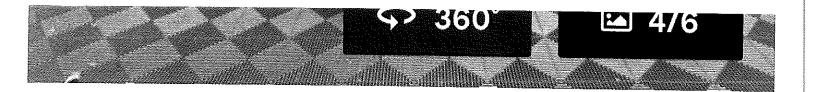
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Result 11 of 50

2024 Honda Odyssey Elite FWD

Nashua, NH (37 mi away)





\$42,987

Great Deal \$1,535 below market (i)

D) Overtime for Operations (\$?)

E) Software Renewal for DataPilot (\$1,000) this allows us to immediately extract electronic evidence from a victim's phone https://datapilot.com

Thank you,

Marc

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THE NATION'S LEADING TRAINING CONFERENCE ON JUVENILE SEX TRAFFICKING

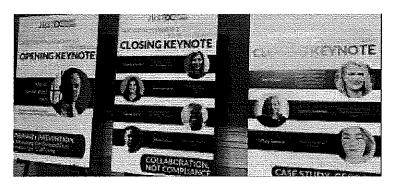
SIGN UP FOR UPDATES

https://www.justconference.org/just-conference-2025/

942000004

This unforgettable and inspiring conference features today's most pressing issues in the anti-trafficking field. Presentations and workshops will focus on skill-building, survivor experiences, cross-discipline collaboration, task force development, case studies and lessons learned. The JuST Conference continues to stand by the motto that each person has a role to play in preventing and ending commercial sexual exploitation and that a collaborative community response is critical. Whether you are just getting started or have been working on the issue for years, we have something for everyone.

Registration will open in the Spring of 2025.



INSPIRING KEYNOTE SPEAKERS Gain insights from the industry thought leaders and y

Gain insights from the industry thought leaders and visionaries who are shaping the future of our work.



MULTI-DISCIPLINARY INTERACTIVE WORKSHOPS Dive deep into the most current trainings and practices to sharpen your skills and advance your knowledge.



NETWORKING & SOCIAL EVENTS

Forge new connections and expand your professional network in a dynamic, vibrant and inclusive atmosphere.



EXHIBITOR SHOWCASE

Explore innovative products, services, and solutions from leading partners in the anti-trafficking industry.



Home



Every day along our highways, in our cities and communities, people, including our children, are subject to forced labor or are illegally bought and sold for commercial sex. However, every day, members of the transportation industry are in these same places ... and in a position to make a difference!

You have the opportunity to be an everyday hero on the frontlines of this issue. Join us in this important work and help us get pimps and traffickers arrested and victims recovered. Enroll in TAT's free, on-demand courses for transportation industry professionals to learn more about the crime of human trafficking and what you can do to help end it.

WE EMPOWER THE TRANSPORTATION INDUSTRY TO COMBAT HUMAN TRAFFICKING.

About

Modern-day slavery, or human trafficking, exists whenever people are bought and sold for forced labor or commercial sex. Around the world, it is estimated that there are over 50 million slaves today. Human trafficking has been reported in all states and provinces, and the number of victims is estimated in the hundreds of thousands.

While illegal, human trafficking is a booming business. Traffickers recruit out of our schools, online, in shopping malls, as well as the streets and other locations. A large percentage of the people trafficked are women and children. Many of them are used in the sex industry. They are the prostituted people on the street, and in private homes and in legitimate businesses, such as restaurants, truck stops and motels. They need to be identified and recovered.

Established in 2009, TAT (Truckers Against Trafficking) started with a simple, yet powerful, belief: that every truck driver can be a crucial ally in the fight against human trafficking. Since then, TAT has grown to encompass a greater portion of the transportation industry, forming deep alliances to maximize collective effort through programs that include innovative training, partnerships with law enforcement and advocacy efforts. Using targeted systems change as our model, we've raised awareness about trafficking, who it targets and its signs, and equipped individuals to take action whenever their lives intersect with it and its victims.

At TAT, we're driven by the belief that together, we can make a difference. Our focus on collaboration, education and action fuels our mission to end human trafficking and restore dignity to survivors. Join us as we work to dismantle trafficking networks and bring perpetrators to justice.



2026 City of Boston's Mass Gaming Commissions Block Grant Proposal

Date Activity Responsibility MGC Law Enforcement Program Coordinator begins July -BPD's: October his/her employment with the COB/BPD/ORD; • ORD's MGC Law 2025 -Program Coordinator works with all internal BPD Enforcement Program Bureaus, Units and Offices to determine who should be ongoing Coordinator & Director required to join and attend the Encore Casino Public The DCU, HTU, • Safety Task Force meetings; YVSF, Area A1 & A15 Program Coordinator works with the MGC to acquire and the FAU, and the contact names, titles, contact email of those law BFS's CVEU enforcement leaders that should be invited to join the Finance Unit **Encore Casino Public Safety Task Force;** Program Coordinator sends out: a memo describing the mission of, and need for a quarterly law enforcement coordination and resource sharing meeting; a schedule, a venue and other meeting materials - (i.e. an agenda, a description of all BPD Units and their Commander's contact information, etc.). *During Year 1, the group may decide to meet monthly. The BPD Units that have received funding in past years will continue implementation of their public safety work with and for the Encore Casino, and document all operations moving forward for reporting purposes. Quarterly program & financial reports are prepared and submitted by the Program Coordinator. July -2026 Letter of Award is sent to the City of Boston (COB) MGC & the COB: Budget Revisions are requested by the MGC and revised December . Mayor's Office of 2025 by COB agencies Intergovernmental New Budgets are approved Relations & Budget Contract & Scope of services are sent to COB to be signed Office COB agencies seek City Council approval for grant funds **Boston Police** • City Council approves grant program Department (BPD) City agencies set up budgets internally within their Other COB agencies agencies, and within COB Budget Office funded City Council • City Hall Budget office and BPD Finance Unit The ORD MGC Law Enforcement Program BPD: January, 2026 - June Coordinator works with: ORD ٠ 30, 2026 BPD Units that are ordering Vehicles & Equipment The DCU, HTU, acquire updated quotes for the BPD Finance Unit, to use and YVSF, Area A1 & to prepare purchase orders and requisitions, so that ongoing A15 and the FAU, purchases can be made. and the BFS's **CVEU**

BPD Public Safety Strategy Timeline:

	BPD Units that are receiving overtime (OT) acquire OT codes so that we may track individual unit activities, and the types of activities that they are participating in, from traffic enforcement, to surveillance to investigations. Six out of 7 BPD Units will be given a separate OT code; The Program Coordinator monitors use of the OT with the Unit Commanders, for reporting purposes; Quarterly Reports are submitted by the Program Coordinator	 Finance Human Resources
January 2026 ongoing	Vehicles and Equipment are purchased and are actively utilized in various operations OT is being actively utilized Quarterly reports are being written and submitted by the Program Coordinator	 ORD The DCU, HTU, YVSF, Area A1 & A15 and the FAU, and the BFS's CVEU Finance Unit
November 2025 on- going	The ORD Program Coordinator convenes <i>quarterly</i> public safety meeting with all members of the Encore Casinos Public Safety Task Force, prepares meeting notes, and staffs the follow-up on strategies planned. The ORD Program Coordinator works with Task Force members to create a MOU amongst LE agencies who serve on the Task Force. The DCU, HTU, YVSF, Area A and the Fugitive Apprehension Unit attend the Encore Casinos Public Safety Task Force meetings, while continuing to fulfill their Unit's mission in partnership with other law enforcement partners that serve the Encore Casino. The Program Coordinator continues to submit quarterly program & financial reports to the MGC; provide crime data to MGC Researchers and participates in research work underway with the MGC research team. The Program Coordinator closes MGC grants at which time it is appropriate to do so, while leading the application process for future MGC grant programs.	 ORD The DCU, HTU, YVSF, Area A1 & A15 and the FAU, and the BFS's CVEU Finance